



75th Edition
Nov. - Dec. 2015

The Coastal Passage



The Joshua C

photo by Bob Norson

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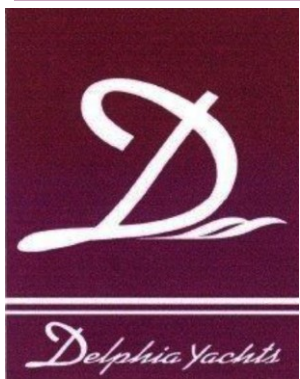
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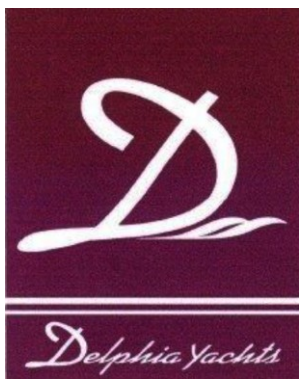
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DELPHIA YACHTS and POWERBOATS

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Allan says: "The new Escape 1100S motor cruiser has a unique advanced hull design. Every planing hull I can ever think of, for the last 50 years, has a dead straight keel/bottom running aft once you get past the curvature of the stem. This boat has a flat run from about midships - the planing area - then curves upwards a little as it goes forward, to an almost plumb bow. Quite a lot of the hull up out of the water at speed.

This hull intrigues me. The under water bow is very fine, as are the displacement models, so at displacement speeds there is hardly any bow wave at all, and therein is, as I say "a very easily driven hull." This of course will relate to good fuel economy, or to put it another way, no necessity for big horsepower engines."

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The cover photo:

The Joshua C anchored at Middle Percy Island.

photo by Bob Norson

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Contributors



***What's your story?
it can't be about you
without you!***

Stuart Buchanan, SY Pluto
Dianne Challis, SC ELAN
Catherine Connolly, SY, Madrona
David Hawkins, SC Quoll II
Leon Morcom, (EX) SC True Blue
Bob Norson, SC Scrappy
Keith Owen, SY Speranza
Susan E. Smith, SY Moonbird
Jan Wooller MV, Yawarra II

As always, TCP very much appreciates your letters and other contributions that provides the rich forum of ideas, issues and news. For information on feature contribution requirements and awards, see the TCP web site: "contributions"

The "Issues" Issue

This is a collection of articles from TCP #15 to #57 that illustrates the advocacy and educational thrust of the paper's content. This is by no means an exhaustive or complete assemblage. It is a sampling and reference to the high points.

TO DOWNLOAD PDF CLICK HERE:

For more see www.thecoastalpassage.com/issues.html

The Coastal Passage 75, November - December, 2015

TCP Editorial by Bob Norson

Shoalwater Bay - AKA The War Games Coast

The boats were piling up at all the anchorages north of the war games. When the wind came around to the east (versus 15-20 on the nose) they all charged along to beat the closures. Just in time to make it nearly useless, the military decided they didn't need to lock up the anchorages anyway.. horses gone... shut gate. Thanks to the military for proving that they didn't need to lock it up in the first place. It was apparently frivolous. Lets hope next year is different.

Leon's Cheap Cat plans and pictures now *FREE!*

Simple plans for a cheap cat were a popular

item when TCP and the builder produced them a few years ago. They were only \$150 when sent out and the material is basic but enough to get a clued up amateur a nice little cat. The builder now says he is willing to give them away. So... in this edition are a set of drawings and basic instructions. If these plans aren't good enough for you to build, you shouldn't be mucking around with a project like this anyway.

New faces!

The migrating fleet was significantly larger this year. Many new faces. Boats are selling but at reduced prices and the new guys aren't dumb. They are looking for value and they like cats. It wasn't long ago that cats were the minority but no more.



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Bob Norson: sometime publisher, editor, journalist, advertising, photographer, etc...

Kay Norson: senior volunteer, ETCF format organizer and semi - retired postie.

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Australian government officials engaged in people smuggling?

By Robert Norson

According to Amnesty International, an asylum seeker boat bound for New Zealand in May this year were intercepted by an Australian naval ship and an Australian Border Force vessel in *international waters*. Australians are alleged to have paid the crew of the vessel \$32,000 in US \$100 bills, (see photo at right) to return the asylum seekers to Rote Island.

After interviewing all 65 passengers who were on board the ship, as well as the six crew and Indonesian officials, the Amnesty report concluded "all of the available evidence points to Australian officials having committed a transnational crime".

Immigration minister Peter Dutton is reported to have said the government had already rejected the report's allegations. But according to the Sydney Morning Herald he wouldn't comment on any of the facts of the matter. A researcher on refugee and migrant rights with Amnesty UK, said evidence showed government officials were allegedly paying a boat crew, providing fuel and materiel, and giving instructions on where the boat should be sailed. "People smuggling is a crime usually associated with private individuals, not governments, but here we have allegations that Australian officials are not just involved, but directing operations. When it comes to its treatment of those seeking asylum, Australia is becoming a lawless state."



A crew and the \$32K USD photo by Indonesian Police

Those on board said the ship was well-equipped and that no distress signal was sent at any time.

The majority of asylum seekers were lured aboard the Australian Border Force ship after allegedly being told they could bathe on board. Once on board, however, they said they were held in cells for several days, before they were transferred to two smaller boats and instructed to sail for the island of Rote. One boat ran out of fuel, forcing all of its passengers onto the other. That boat foundered on a reef at Landu Island, near Rote, from where locals rescued the passengers.

On the original boat, the six crew claimed Australian officials gave them \$32,000. Two of

the men received \$6,000, four \$5,000, in exchange for the crew agreeing to pilot the boat back to Indonesia.

Anna Shea of Amnesty International stated the 62 passengers from the vessel were interviewed, as a group, on three separate occasions in Indonesian immigration detention in Kupang in West Timor, where they are currently being held.

The six crew, who are in police custody on Rote Island, were interviewed separately to the passengers. "What was really remarkable was the degree of correlation and consistency in the testimony of the

asylum seekers and the crew, who were held in different locations, and who were not in communication," Shea said.

After initially refusing to comment on the allegations, citing secrecy over "on-water matters", the Australian government denied making payments to people smugglers, and said Australian officials acted to save life at sea.

TheGuardian.com/au reported in March [Australia has a multimillion-dollar contract](#) with a Vietnamese ship-builder to manufacture fishing boat-style vessels to be used to return asylum seekers to their countries of departure, usually Indonesia or Sri Lanka.

see commentary, next page..



The Criminal Culture at Customs, Border Protection

Our governments agencies have turned into the 'go to' mob when you want a thing done quietly, illegally and savagely. What our forces are doing to the asylum seekers should weigh heavily on our consciences. They weren't a problem until we started blowing their country up. We are the ones that have sent bombers over there.. and troops.. and that's just what we know about!

Our forces have detained a foreign flagged vessel on the high seas who's destination was another country, seized passengers and threw them into jail cells. Isn't that Piracy and Kidnapping??

TCP joins The Guardian and Amnesty International in calling for a REAL, HONEST AND INDEPENDENT ROYAL COMMISSION WITH BROAD POWERS to investigate these serious charges. If such an enquiry were to take place I am certain that episodes like that reported in this edition would be but a tip of the iceberg of crime

committed by our bureaucrats and forces. In fact I know that personally and would be keen to testify at such an enquiry.

But will it happen? Doesn't look good so far. Currently there is a senate enquiry going on which is little more than the governments white wash machine. Ten to one against.

Who do you believe? Us or those foreigners?

Will the new PM make a difference? An outside chance. He had leadership before and made a stand on principle and got bounced out of leadership so fast he left a sonic boom on his way out. You do not get in the way of the real powers that be, the handful of people/corporations that really rule both parties, without consequences. Turnbull would need a tsunami of public support to get away with it. A demonstrated mandate.

Here is the way it works; The powers that be, lets refer to them as TPTB, want a thing done. The government agency best equipped gets it done, no rules of engagement. OR; a party that is favoured by TPTB wanting to acquire or retain power wants a thing done, it gets done. The

politicians will not want to know how it is done. The wink and nod agreement is, the forces will hide behind the veil of secrecy provided by the politicians. The politicians will swear the forces have not done anything illegal and will run interference for them, contrived enquiries and declarations of innocence for the forces. 'Who do you believe? Us or those foreigners!' There will not be any explicit agreement, no document likely to indict the polities. If somehow the truth is uncovered, in this case by the actions of foreign police and Amnesty International, and the polities can't get it smoothed over, even with the help of their mate Rupert, then someone in the forces will have to fall on their sword. Take full responsibility. Resign in 'disgrace' and pick up a very fat job as a 'consultant' in one of those corporations of TPTB.

We only know this because of the actions of the Indonesian Police.

Do these agencies operate domestically? YES. Don't our laws protect us from interference as long as we aren't doing anything wrong? NO

These agencies have demonstrated before that they have no regard for our own or international law as stated in the Amnesty International report; "Australia is becoming a lawless state."



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Bob,

I just downloaded TCP and was stunned by the pictures and news of the dismasting. I can fully commiserate with you having been there myself about 30 Years ago in the Molokai Channel.

We were headed to the big island to see a total eclipse of the sun as where we lived On Oahu you would only see 90%+. It was a perfect day, 3m swells, 15 knots of wind. But too much sail (150 genny) for my (unknown to me) compromised swage fitting at the deck end of my port capshroud. Like you, I heard a loud bang and then saw the top half of my mast as if in slow motion take a header into the ocean.

Pretty much the same thought process as you - re-orient to your new reality and figure out how to salvage the situation. We were on a 36' keel boat with a conventionally rigged, keel stepped, single spreader mast. This left us in a much better position than you were in.

We had plenty of sea room being 10+ miles from land. The mast though moving around, presented no immediate danger to the hull. But still when my wife went for the 8mm video camera (remember those?), I couldn't believe my eyes and told her this was not the time nor place for movie making. Undeterred, she continued to pan the scene stopping just short of my mental/emotional tipping point.

Notice to contributors: All contributions that purport facts in a matter of possible contention, should be ready to provide support for their assertions or the contribution may be refused at the discretion of the editor. Anyone disputing a matter of fact in any part of TCP is **invited** to respond as long as the discussion remains one of fact and the responding writer must also provide support for their assertions. Personal attacks will not be published and rude or offensive mail will not get a response.

Hardest thing was to get the genny back aboard. We spent probably an hour or 2 (as they say--'the longest week of my life') getting it and all the standing/running rigging out of the water.

My mast configuration resembled an 'a' with the masthead 3-4 feet under water on the starboard side. The break (crimp really) was (as yours) a couple meters or so below the spreader. The lower part of the mast was secure through the deck in it's step on the keel. It was fairly easy to lash and secure the top of the mast to the slotted toe rail of the gun'l.

Once assured that all lines were out of the water we fired up the engine and motored back to Kaneohe Bay & our anchorage arriving well after dark. With the help of friends the mast was repaired--to long a story for now. Maybe we can swap tales later over some mackerel and James Boag while swatting sand flies on an Aussie beach when your recent events have been numbed a bit by time.

You will be an even busier guy for the for-seeable future, so do not feel obligated to respond to my emails. Just know that we hope you are back out on the water soon and kudos to your insurance co. I don't have much faith in those guys but sounds like you picked the right mob.

Don't worry bob 'she'll be right mate' (I couldn't resist).

David Goodgame, SC *Bobcat* (California)

More LETTERS

TCP HAS THE MOST INTERESTING READERS!

Last issue a handful of people didn't get their notice of publication due to a small glitch. We value every reader so... I manually sent each a email notice with an apology for being a little late and received the following:

Thanks Bob,

I was already wondering. Great material you are sending out, go on, please.

**Regards,
Frank in Brazil**

Cheers Frank in Brazil....

*Wait a minute! Brazil!! Frank?!?..
expat from where?*

Cheers, Bob



Franks boat, Zip

Hi Bob,

I am German but left that country at age 10. Since then, I lived in Peru and Brazil (total of 63 years) with some time in between in Argentina and 18 months in Germany (too cold there, I ran from the climate but fished a nice wife there before that).

In Brazil I am since 1970 and always close to the sailing communities. Presently, living in the interior of São Paulo State. I am in a group/fleet that works developing the Tietê River (1100 km, the largest of the State) for tourism and fun. I sailed that river 10 times in all its length in a group of boats that is growing constantly. The last fleet, in July, was formed by about 25 sailboats. The Tietê is somewhat unique: The source is close to the city of São Paulo, 18 km from the Atlantic, but it flows inland, finally becoming a tributary of the Paraná that ultimately encounters salt water between Uruguay and Argentina, then called Rio de la Plata.

The boat I use mainly is a Wharram Tiki 26 that was on the cover of a leading sailing mag here. That veteran of 30 years keeps me busy repairing all sorts of rotten parts. My Guru in that is Dave Carnell. My racing times are long over, good memories racing Lightnings, Finns and Lasers.

I keep one of each, the first 2 of wood.

**Best Regards,
Frank, SC Zip**



Samal Island not safe?

Hi all,

We have just received a link to the recent Youtube clip of the yachtsmen who were taken hostage in the Philippines recently.

Hard to watch as the poor things are obviously terrified. All we can do is spread the word that this area is not safe and should be avoided.

I'm sure we all have friends who've been in the area in the recent past.

**Hope you are all safe and well.
Cheers,
Nick & Jan Wooller**

Hi Jan and Nick,

Do you think we should put this in letters?

Cheers, Bob

Hi Bob,

Yes. *(put in letters)*

Although the BBC keep talking about them being taken from a "resort" the kidnappers arrived by boat into a "secure" (from land!) Marina and took all 4 from a yacht tied up in the marina.

Our friends who sent us this info spent many months in this marina a year ago and thought they were safe because it had "good security". Don't think they will be returning to the **Holiday Ocean View Marina on northern tip of Samal Island near the city of Davao.**

Bottom line is that this whole area is NOT safe.

**Cheers,
Jan**

Pacific Pearl Found!
Published previously:

Hi There,

A very long time back when I was about 17, in Mackay, I was the boyfriend of one Sari Cummings. She was the daughter of Doug Cummings, an ex-Broome pearl diver who operated a trochus lugger out of Mackay. The boat's name was (then) *Sari Rizah*, and my (then) girlfriend was named after it, or the other way round :-), I can't now recall exactly.

I found a few pix of the boat, and a partial article saying she had later become the *Pacific Pearl*. I also found an article on the sale of the *Pacific Pearl* in 2010, I think after the owner had died.

There was mention in another article about '*The Islander*' (TCP website > *The Percy Saga*) of the *Pacific Pearl*'s owner's wife living on board. Can you please update me as to where the *Pacific Pearl* is now?

**Regards,
Les Elmer,
Auckland, New Zealand**

Hello,

My name is Tom and *Pacific Pearl* is my boat. We recently went on our first extended cruise around southern Moreton Bay and Jumpinpin areas. We put a blog up each night on facebook. I don't know how to send you our trip but if you find me on facebook you can have a look and use anything you like. Start from day one.

**Thank You,
Tom Whatley**



A SURVEY FOR DREDGING THE LABRADOR (GOLD COAST) CHANNEL

Last week Aurecon Group quietly sent out a Survey, which will, in part, represent the findings of the Minister's Independent Enquiry into the dredging of a Western Channel - the so-called Labrador Channel Extension.

Conspicuously absent is background information which would allow any stakeholder to make informed comment.

So, we've added some background, which represents our combined research over many months, trying to get to the real truth of this "2-Channel Strategy".

Please follow the link in this attached document and after reading the short background briefing, complete the survey as a concerned (and informed) stakeholder.

**Regards,
Your Committee
Australian Live-aboard Association
contact@australianliveboards.org**

**Please follow the link and complete the survey.
There are only 18 short questions.
<https://www.surveymonkey.com/r/GCWALCE>**

The answer to question 15 is "The Labrador Channel extension should not proceed"

BACKGROUND:

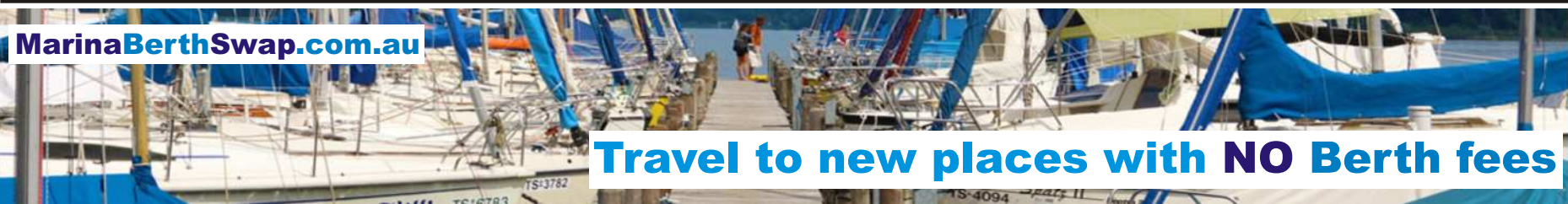
The Gold coast Waterways Authority (GCWA) are proposing to dredge a channel from the Sundale Bridge through the 6 knot zone to meet the recently dredged Labrador channel.

The GCWA originally claimed that this will provide a slow lane for vessels heading north to the Gold Coast Seaway and beyond. They say it is necessary to reduce congestion, reduce marine incidents and improve safety in the existing main (south) channel.

The problem is that there are no congestion issues or related marine incidents or safety issues that require a 2.5m deep channel to be dredged through the existing 6 knot zone.

The total cost of the project is estimated to be in excess of \$5M and no speed limits have been revealed for the section beyond the 6 knot zone or in the already dredged section.

continued next page...



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A SURVEY FOR DREDGING THE LABRADOR (GOLD COAST) CHANNEL

IMPACT:

36 moorings (with potential for 40 more), comprising the largest and last viable mooring field in the southern Broadwater, will be removed.

Sea-grass stepping stones (used by juvenile fish species and prawns moving from south of the Sundale Bridge to the Gold Coast Seaway, will be destroyed with increased shore wash, less clarity and disturbance), which will have a major impact on recreational and commercial fishing industry.

Silting of the existing main channel will become more rapid due to splitting the tidal and storm water flows resulting in less scouring of the channel which will result in increased dredging less clarity and further damaged to remaining seagrass meadows and the benthos.

Loss of one of the best windsurfing and kiteboarding amenities in Australia. Any further increase in vessel movements in the Labrador channel will increase safety risks to the existing users.

The channel will pass straight through and render useless an area used for training jetski operators.

Stand-up paddle boarders, kayakers, surf club nippers and other small craft already use the mooring field (destined for replacement with the second channel) as a barrier from vessels travelling in the existing 6 knot main channel.

The survey is titled "Community Consultation Survey" but fails to provide any relevant background information to allow informed answers..

In answering the questions be careful of the following:

Question 5: The area occupied by the proposed extension of the Labrador channel will remain a 6 knot zone. No good for jet skis etc.

Question 6: There are no safety risks in the existing channel that require a second channel.

Question 7: Search the web of the many benefits of seagrass which should be protected and remember reduced flow with a second channel will increase silting and require more dredging.

Question 8: Increased vessel traffic through the Labrador channel will require speed limits to be applied and will impact on and possibly exclude the existing users.

Question 9: The removal of 36 existing and a possible 80 moorings will impact on chandleries, boat sales (low cost, safe, vessel accommodation). Increased traffic in the Labrador channel will exclude or limit the activities of many existing users including Jet skis and windsurfers. The second channel will also adversely impact on the fish habitat.

Question 11: There are 500 names on the mooring waiting list with a 10 year waiting time and they want to remove moorings!!

Question 15: "The Labrador Channel extension should not proceed".



More

LETTERS

Hi,
Hope this email finds you well!

Would you be interested in reaching out to "Boat Owners Email List" with opt-in verified contact information across USA? By which you can expand your reach and widen your client base.

We also have contacts of RV Owners, Motorcycle Owners, Harley Davidson Owners, Adventures Travellers, Pool Owners, Car Owners, Luxury Goods Buyers, Horse Racing Enthusiasts, Business Owners, Chocolate Enthusiasts, Home Owners, New home owners and many more.....

We also have data for Contact Name (First and Last), Address, City, State, Zip, County, Opt-in Email Address,

Boat use, Boat length, Boat Propulsion, Boat Fuel, Boat Hull Material, Boat Make, Boat

Hull Shape, Boat Size, Boat Year, Boat Transaction Date, Boat Transaction Type, Boat Validation Date and Registration date.

Please review and let us know your thoughts toward using or procuring our Boat Owners contacts.

Thanks & Regards,

Research Analyst

If this message is not relevant to you please forward to decision maker...

TCP receives mail like this all the time!

We insert this letter to make it very clear to you readers, that we do not sell, loan or handle our mailing list carelessly.

If you receive mail generated by the lists the above party sells (and there are many businesses like this) then someone is selling you out.

Another source are merchants that collect email addresses with promises of discounts or other rewards. But when it is directed at a persons special

*interests, boats, Harley's, etc. It is very often magazine subscriptions or **website registrations**. And if you ever fill in a "survey" for that site or mag.. well, do you really think that info will be used to improve "your viewing experience" or "product enjoyment"? whamo! On the list sucker, you've been sold out..*

Social media is probably the number two of personal data collection. Google is number one.

*TCP advises to take an interest in protecting your privacy and appreciating those that respect it. **Privacy is the new gold. Those who give up their privacy now give it up for good.** Those who guard their personal information and have access to information of others will have a powerful edge. Those who give it up are disadvantaged. Think about your future.*

To be notified of new editions of TCP send a message to this address from the address you wish to receive notice on.

notice@thecoastalpassage.com

Regards,

Bob n Kay



Come on down! Carefully...

Hi,

Just want to tell a good story about Australian Customs and Quarantine. I was sailing into Australia for the first time from Indonesia with a lot of apprehension about our Australian Customs and Quarantine after many stories from yachts. Stories enough that a lot of yachts don't come because of such experiences.

We sailed into Gove early November 2014. Had our booking with Customs and Quarantine organized as required. Tied up on the dock when required and met the men from Quarantine and Customs. I can say it was lovely to deal with these people. Paper work was efficient; inspection was carried out with minimum of fuss. Well the boat was tidy for a change so that helped.

We had an ongoing issue with quarantine and they worked with us to solve the problem. After that issue was solved we went through the procedures to import the boat. Another paper trail but all went well. I mainly dealt with Cairns Quarantine and found them thorough in their work and always courteous with the aim to do what they have to do but with us in mind also.

Customs dealings are spread from Gove to Cairns to Brisbane, all helping to make the paper work move through the system. Thanks to the team who had to deal with us and to the yachty's, COME ON DOWN.

Regards Geoff Hazelton, Heather May

Greetings Geoff,

First of all, you are welcome! I'm sure you were going to say thank-you TCP for bringing about the change in Custom's activities and in alerting the world cruising community to our draconian laws that are still unique in the world if you exclude China and North Korea and the like.

continued next page...

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I would also like to mention how TCP brought about the stop to the illegal random searches and interrogations of yachts in domestic waters. Yachts from other countries would otherwise be quite surprised at that kind of STASSI like behavior, maybe even a little excited at the adventure of it all. But not to worry that they miss out as now what Customs do if they see you doing something suspicious, like crossing a bar at night or other nefarious activity, they ring up their mates at water police and send them out on a bogus, "safety equipment inspection" that involves tossing the contents of the vessel whilst a couple of those nice Customs people come along for the ride and observe.

Yes, for those that still chase the excitement of a police state, we are not without our thrills! I can't wait for international visitors to get fined and booted out of Queensland's most popular anchorages like Mooloolaba or the Gold Coast (see letter from ALA). And just wait till they get fined for using their dinghy for site-seeing or fishing and other things tourists do (see TCP #73?). That'll be a hoot!!

We are always pleased to see these letters come in. Sometimes from real yachties that think because they had one positive experience they know the whole story. But surely of good intent. The other times from government agents or cronies working to exploit letters columns in publications to manipulate public perception. They even have a name for it, "Astro-Turfing", a reference to the synthetic grass used in sports stadiums. Grass roots-synthetic grass, get it? So clever! But at TCP, we are stuck with the truth, the facts, all of them. We want visiting yachts to still come to Australia, but will not be a party to handing out half-truths to entice them into what could be a negative experience. Come on down! Carefully...

Cheers, Bob

DRAMA ON THE HIGH SEAS

From Island Cruising Association's John & Lyn Martin on SV *Windflower* Pacific Circuit Rally, courtesy of Andrew Keys

When we started off from Fiji on the Pacific Circuit Rally leg to Vanuatu we had waited a week to let a Tropical Depression in the region dissipate, but the seas were still lumpy. The forecast was for the weather and sea state to improve so we boxed on, at least that was until we received a Pan Pan via email!

Windflower Arriving on the Scene

The email came in with the evening position reports saying *Sweet Disorder*, a Lotus 1280, had suffered damage to their steering and were taking steps to rectify. New Zealand (NZ) Maritime Radio had been advised and we stood by for more information. By 2230 hours that evening the situation had deteriorated, so we turned *Windflower* around and headed back to stand by in case assistance was needed.

At 1100 hours the following morning we were standing off *Sweet Disorder* who had drifted overnight some 26 miles in a roughly northerly direction. *Sweet Disorder* was entirely without a rudder and all efforts to rig a jury steering had failed not because they didn't have the gear aboard but because the forces on the jury rig were beyond what it could handle.

COMMUNICATIONS

We put the Iridium GO sat-phone to good use during this period, with calls to NZRCC, Sweet Disorder and Fiji. I was expecting a huge bill but was very surprised to find afterward that we had been automatically upgraded to the new Unlimited plan which now includes 150 monthly talk minutes rather than the previous 5 minutes. Rescue Coordination Center in NZ (RCCNZ) got the ball rolling with Fiji Rescue Center in Suva and then effectively stepped aside. As a Pan Pan only had been issued and there was no imminent risk to life and Fiji had directed a fishing vessel to the area; it was now deemed a recovery operation not a SAR.

continued next page...

THE RIGHT PEOPLE IN THE RIGHT PLACES

I can't say enough about the people involved in this incident. Island Cruising Association is associated with many companies in the area, chosen for their ability to smooth the way for the various rallies around the Pacific. It is gratifying that the planning works and our friends have come through for us.

First up is the Skipper and crew on *Sweet Disorder*, cool, calm and methodical. Full marks to Paul and Helen Leydon and their crew. Fortunately, *Sweet Disorder* was insured. Baileys in Auckland are the brokers and talk about stepping up. Neil has sent in some comments below.

We received an email from the NZ Search & Rescue coordinator on Saturday, late afternoon advising us of the predicament *Sweet Disorder* was in, and asking us will their insurance policy cover the cost of a 150-mile tow back to Fiji? This from Neil Bailey: "We only had sketchy information at this point. We could appreciate the position and state of mind Paul and crew would be in, so we had to put our b***s on the line, and give a definitive answer -YES!"

Neil was involved again with the discussion on destination and finally again when *Sweet Disorder* got to within 10 miles of Navula Passage and a second tow had to be sanctioned as the fishing boat refused to go further.

And lastly, Yacht Help in Fiji. David Jameson and his crew liaised with the Fiji Navy, sorted all the paperwork out, negotiated a clearance at Port Denarau arranged the second tow and got the ball rolling with repairs.

The Fishing Boat Arrives - The Seas are Rising STARTING THE TOW.

Now this whole business was not as easy as it would seem. The Fiji Navy had contacted two vessels in the area and directed one "the fishing" to the area. While in transit negotiations were underway as to who was going to pay the bill.

Baileys stepped up and took on that responsibility. They arrived on site and the first thing that became obvious was that communicating with the fishing boat was going to be a problem; a skipper that spoke next to no English and a Fijian crew that had some but limited English that he spoke at a dozen words a second.

At this stage I took over the comms to allow the crew on *Sweet Disorder* to get on with preparing the boat. While waiting for the fishing boat we discussed various scenarios: how to set up the tow; how to securely attach the tow line; and how to stop the line from coming out of the fair lead.

Simply attaching the line to the bow cleats wasn't going to cut it, two reasons: the line the fishing boat was proposing to set up was about a 60mm diameter and secondly: the loads under tow are extreme and the cleats wouldn't have lasted 5 minutes.

After some frustrating back and forth it soon became obvious the fishing boat was intending to tow *Sweet Disorder* to Suva and it took over an hour of backward and forward and calls to their boss to get agreement that Lautoka would be the destination.

Meanwhile Paul and crew had *Sweet Disorder* pretty well sorted. Lines were run around the mast and braced back to the primary winches. Lines were run from the twin bow cleats to ready hold the tow line central and the anchor was removed from the bow.

Meanwhile the fishing boat crew were busy sorting the tow line and bridle out. Two crew were sent onto the roof of the aft deck with a Monkey's Fist and light line. By this time they were rolling in the building seaway. The boys showed amazing balance almost like it was a walk in the park. The skipper of the fishing boat showed his skills by maneuvering his vessel within throwing distance; it looked close from *Windflower*, some 200m away. It must have loomed huge from *Sweet Disorder*. On the third attempt the light line was passed and then used to haul the main line over.

continued next page...

DRAMA ON THE HIGH SEAS

Getting the line through the Pulpit and then the fair lead took some time, then attaching it to the lines set up on the bow. All this time the fishing boat remained on station. Now, comms became a problem again with every exchange taking ten times the length it should have. Are you ready? That means will give you some idea.

The line was initially set at about 150m but it soon became evident this was too long. When the tow started the skipper put on too much power and *Sweet Disorder* looked like the tail of a whip as she sheared from side to side. A drogue was set up off the stern and the tow length shortened, a couple of times, eventually finding a maximum safe tow speed of around 3.8 knots.

They were finally on their way at dusk that night with a long slow tow to Fiji. There were some shenanigans on the way too. The skipper was an impatient sort and after dark started to increase the speed. Paul related to me after that it took him three attempts just to reach the mike the first time.

The tow was uncomfortable, to say the least, and the skipper on the second night turned to starboard, deciding Suva was a better bet for them. Slow negotiations again got them heading in the right direction only for the same thing to happen just short of Navula Passage, the main shipping channel into the Lautoka area. The presumption was that the fishing boat had never been there before, it was dark and the skipper most likely didn't have charts for the entrance.

A local game fishing boat eventually finished the tow and *Sweet Disorder* and her crew arrived to a Brass Band and much fanfare at Port Denarau Marina with the Fiji Navy taking all the credit for the rescue, hey, they were safe so where's the harm!

CONCLUSIONS

Sweet Disorder was hauled at Denarau and on investigation it was found the 2 inch Stainless Steel rudder shaft had sheered off at the hull, leaving the remainder of the shaft in place hence no water ingress.

Rudders have long been a niggle area for me principally spade or unsupported rudders. Falling off a wave in the open ocean puts huge loads on the boat and when you look at the lever arm on a 1.8m rudder it's no surprise we see failures.

In the PCR fleet this year we've had four boats with rudder problems, all spade type rudders. *Sweet Disorder* obviously but also two Bavaria 46's and a Beneteau 44. All were hauled at Opuia prior to the start of the rally for rudder work. They're not alone wave seen rudder problems again and again both in the fleet and with other notable cases.

I must admit, I've thought in the past and indeed talk about it in our Cruising Preparation Seminars about how to rig an emergency rudder or steer by a drogue; all very well in theory but this episode has given me a real world look at the problem and in this case reality has lifted a couple of fingers to theory.

With hindsight, it's my opinion there needs to be more done to prepare boats with an unsupported rudder. We already ask Bavaria owners to have their rudder area checked by a shipwright (hence the two haul outs in Opuia) but that may not be enough. The difficulty Paul had setting up emergency steering shows that the old floor board and a couple of u Bolts needs to be thought through with a bit more detail and should be trialed before heading over the horizon.

Rumor has it the tow cost close to \$30K, ouch, perhaps there's a better alternative, and one of my favorite words would be redundancy. I commented to Paul when we met them in Vila, (they had family already booked to join them in Vila and we ended up joining them there too), we often look at either beefing up or adding a second autopilot to the boat, neither one would have helped in this instance. On the other hand, a Hydrovane Wind Steering unit, which has its own rudder, would have. In fact, they probably wouldn't have even noticed the loss of the rudder until the disengaged the Hydrovane ready to motor into the anchorage at Anietyum. Food for thought.

Ship Shape Marine Services now at Mackay Marina!



Denyelle Gillingham (above), owner of Ship Shape Marine Services is now set up at Mackay Marina Office. Denyelle offers interior and exterior boat cleaning, interior detailing, a boat minding service, layoff maintenance, secure storage for excess gear during the racing/cruising season and provisioning - from food to boat parts and services.



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*Spend your time enjoying your boat -
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Denyelle Gillingham
Mackay Marina
0420 909 068
shipshapems@bigpond.com



From TCP #74:

CSC 30 CATAMARAN

Hi,

I just wanted to tell you how much I have appreciated your website, particularly the effort you have put in to share with us the build of your cat. I have really enjoyed reading your informative blogs during the build and it's great to see that you now have it afloat. Congratulations and I wish you all the best with your adventures.

I would love to build my own cat for my young family and believe that Leon's 21k design may be something I could manage for my first ever boat build. I have restored many a ply boat but never built from scratch. Is there any more info / plans you could share with me regarding this design? I have been experimenting with some scale balsa hulls but without any measurements to go by I am uncertain how it will float once built to full size. ***If only I had some bulkhead plans.***

My ideal cat would meet the below criteria:
32 feet long, 16feet wide, with full width

bulkheads. 9mm plywood multi chine hulls with a ply bridge deck with a hard top extending aft to provide shade over the cockpit. Standing headroom in saloon and cockpit. A dividing bulkhead between the saloon and cockpit is not necessary.

It would be very curvey, dissimilar to most currently available boxey designs. Shallow draft with bilge keels and akermann spade rudders. Could be powered by a mid mounted 9.9 hp four stroke outboard.

Provide accommodation for 2 Adults (double bed) and 2 children (separate single beds) Basic galley. Chart table not important. Suitable for weekend stays in our surrounding waters of Goolwa which consists of freshwater lakes and the Coorong estuary. Cutter rig with a simple layout for cruising.

I hope to hear from you and would love to see your boat one day when we come north next time.

**Cheers,
Andrew Stevens**

Hey Andrew!!

I hope you are still reading TCP because we have a little surprise for you. Apparently Leon, the designer and builder of the CSC30, read your letter.

Even if the CSC 30 isn't perfect for you I will bet there is a lot of valuable information in the following pages.

This is the article that started it all >>

Six years ago, TCP published this one page article and started a revolution. Even with today's soft market and higher costs these simple plans can make building your own cat a doable thing. Leon contacted TCP and said; "My days of boat building are over.. so if you wish.. you can put them on your website for anyone who is interested.. it just may help someone get started." TCP also had invested a lot of time in converting the drawings into reproducible graphics but if Leon is OK to give it away, so are we. They were always a hassle for us to prepare for sale anyway. We will mount them on the website as soon as possible and we also include them in this edition.



Leon's \$21,000 30ft + Catamaran!



As first published in TCP # 37

Think it can't be done? A sweet little coastal cruiser in the water and on the cheap. The builder claims about 900 hours and he has done enough of them to know. His first one was in the seventies. The ply is covered in glass and vinylester outside and inside is done in Watty! epoxy and varnish. Just have a look at the photos and see what you think. There are no plans... Leon just draws em for himself. But don't take my word for it... read Leon's notes next page.

continues next page...



Leon's \$21,000 Cat!

Pretty self explanatory but the four main panels are simply sheets of ply with the stringers and bulkhead positions glued and fixed in position (liquid nails and nail gun) then rot proofed and flipped over and glassed and finished on the floor.

The main bulkheads are then fixed to one panel, stood up and the other panel is fixed to them. Pull the bow together and then the stern to the transom. Square up diagonally the main bulkheads and brace. Fit chine stringers and measure and fit remaining bulkheads (cut two at once of everything so both hulls are the same). Fit and cut bottom panel (two again). Now make and fit keels (you could fit daggerboards if you prefer but I believe the long flat surface of the chine gives lateral resistance) as it is easier without the chine panel. Now fit and cut chine panels (four this time). Glass and finish then flip. Fit pre-made crossbeams and do internal fit-out then do cabin/decks/hatches etc.

continues next page...



Wind Generator super deal

Rutland 914i

complete with HRSi regulator

\$1399.00

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Leon's \$21,000 Cat!

Cockpit/saloon is all done on the floor and jacked and bolted in place and then saloon cabin is made and fitted. Sounds simple? Well it really is compared to strong-backs then stringers fitted over frames etc. This boat was designed around the rig that I had bought from a wrecked boat for \$800 and was more an experiment than anything to prove to a couple of people that I probably am WACKED but not afraid to back my self.

I have to admit that the Waller concept had a lot of influence on the actual design but it was drawn up on the back of a beer carton after a few stubbies(still got it somewhere). I think I have proven (at least to

myself) that you don't have to spend your life savings and spend years to get out there and do it. No she is not an ocean crosser, though I have seen worse do it, but as a coastal cruiser she is quick, easy to handle, comfortable and I have come to really like the little bugger! (I think it could be love!).

Now having said that, it's hard to say that's it as there is always that next design eating away at what is left of the brain.

Boatbuilders never retire, they just sort of slowwwwwww down.

Leon



Leon's \$21,000 Cat!

That concludes the original article. Continuing after this are copies of the graphics that give the dimensions of the key components. With those components in place the rest is fit to fill the opening! It is really that simple.

Also included in this article are graphics and instructions for the making a device to get the dimensions for the optional placed bulkheads that appeared in TCP # 74 letters section.



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CSC 30 CATAMARAN

The CSC 30 is designed to be quick, easy and cheap to build.

This being the case it has it`s limitations and is in general terms a coastal and smooth water vessel and should be regarded as such.

In setting out these plans and dimensions I take absolutely no responsibility as to any outcome be it in construction or use of the vessel.

These plans are intended for a person of sufficient skill to work independently but who may benefit from the experience of the original builder to save time, money and insure outcome.

STEP 1

Lay out four (4) sheets of 1200x2400 x9mm ply and join using half lap joins then set out all stringer and b/head positions and the tapered cut from rear main b/head to

transom and the topside curve to top of transom.

Now glue and fit all stringers and b/head stops in position. (18 gauge stainless brads and construction glue work OK)

Any scarf joins can be done in place.

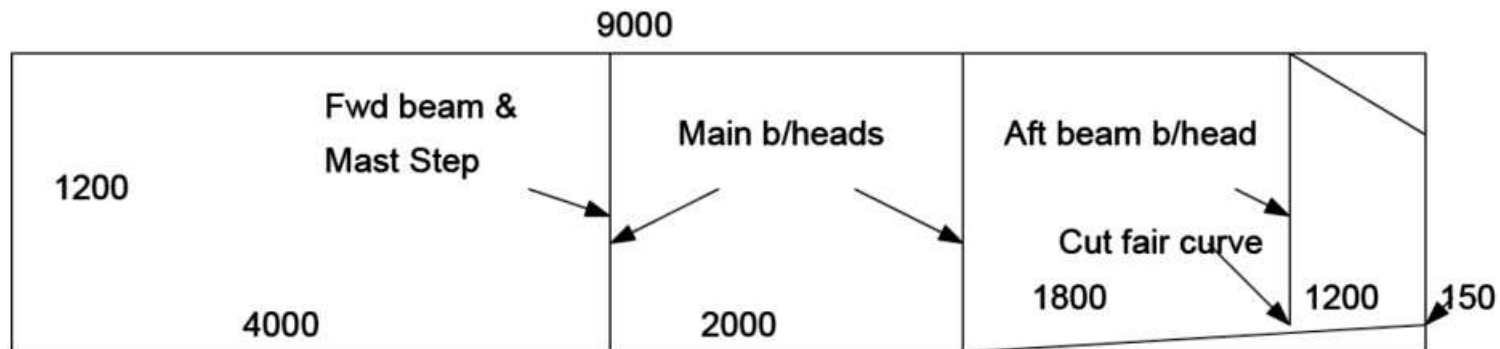
Now round off all timbers with router where required and treat all with epoxy wood preserver.

Turn over and sand and glass with 300gm mat glass and vinylester resin and sand and finish .

Repeat this process with remaining side panels remembering to make two pairs of panels.

See next page for sheer panel plan

MAIN PANELS cut 4 off 9mm ply (2 pair)



Ply can be butt joined or half lap.

Intermediate b/heads to be measured and fitted after initial assembly.

Intermediate B/head positions can be set to suit layout

Aft beam b/head position not critical.

All stringers and b/head positions to be fitted and glassing to be done before assembly.

CSC 30 CATAMARAN

STEP 2

Cut out the four (4) main b/heads as per dimensions supplied and the two (2) transoms and cut notches to suit stringers.

Fit two main b/heads to one of each main panel of each pair and stand up on level surface and fit matching panel then pull stern in and fit transom and bow together clamp and glue.

You now square up diagonally the main b/heads and brace and fix to floor.

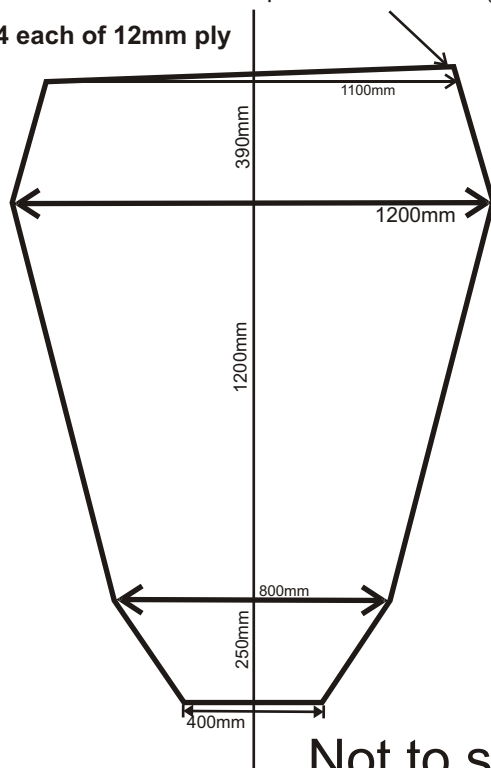
Fit the two bottom chine stringers and measure and fit remaining b/heads.

Intermediate bulkheads should be full/half or frames and placed at 1000mm centres or to suit fitout (bunks etc) as position is not critical but must be worked out prior to commencement of construction so stops can be fitted to main panels before glassing.

Main Bulkhead

Cut 4 each of 12mm ply

plus 50mm camber for decks(optional)



Not to scale

CSC 30 CATAMARAN

STEP3

Measure and fit bottom sheeting (12mm ply) ensuring to keep a centre line matching ply and b/heads (cut 2 at once)

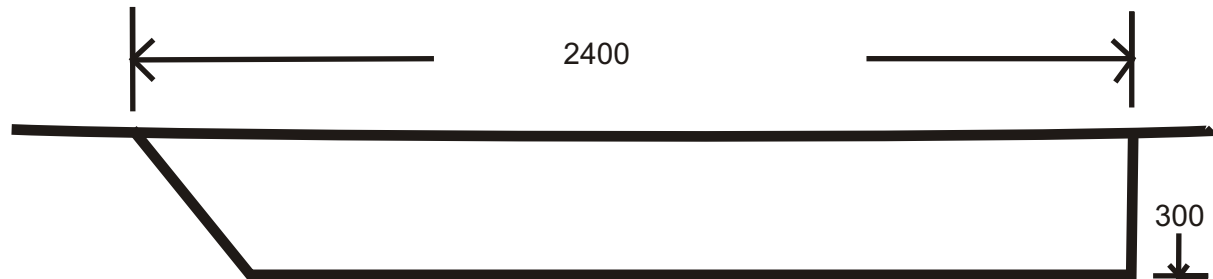
Now make and fit keels (see drawing).

Measure and cut the remaining bottom sheeting (12mm ply)

Plane and sand then glass with 600gm mat glass and vinylester resin with extra layers on and around keel.

Extend 600 gr glass to 100mm above sheer panel joint.

Keels



Use solid timber 50mm thick and match curve of rocker in keel panel. Install 5 coach screws (lag

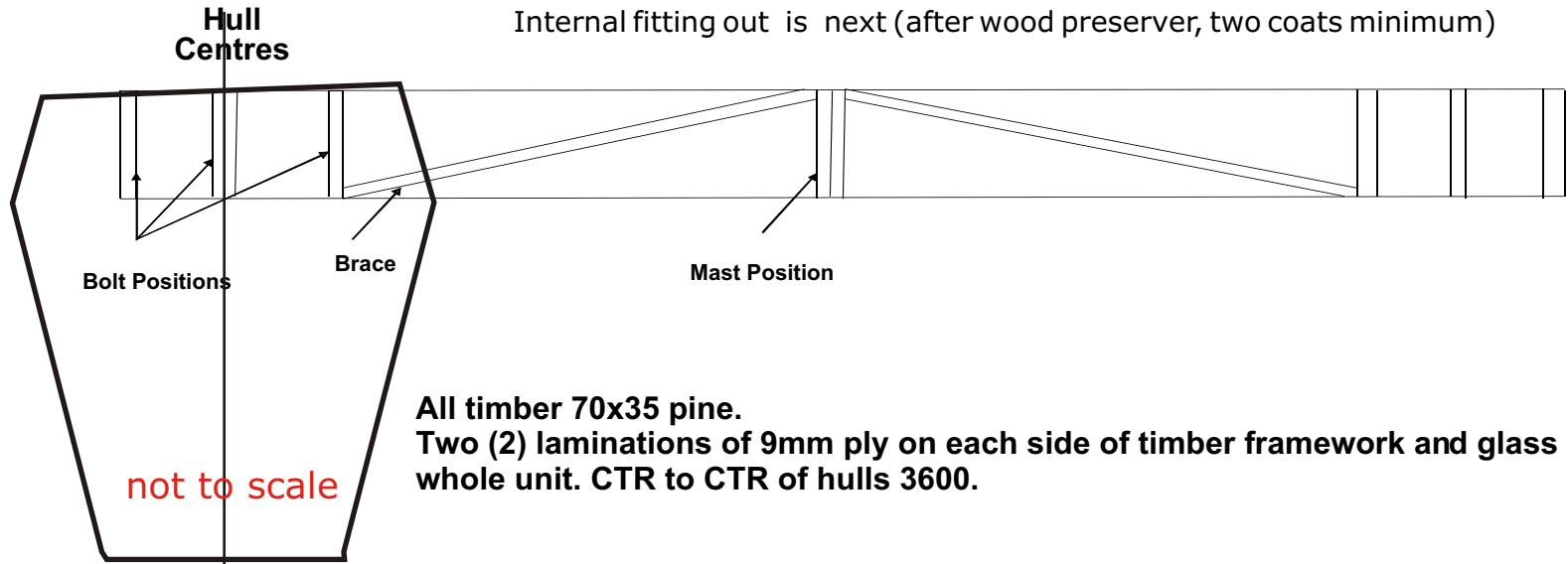
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STEP 4

Fill, sand and finish to priming point then make and glass the beams.

Mark centre lines on both beams and beam b/heads. Pre drill beams for fixing bolts then turn hulls and position and fix beams making sure everything is level and square.

Internal fitting out is next (after wood preserver, two coats minimum)



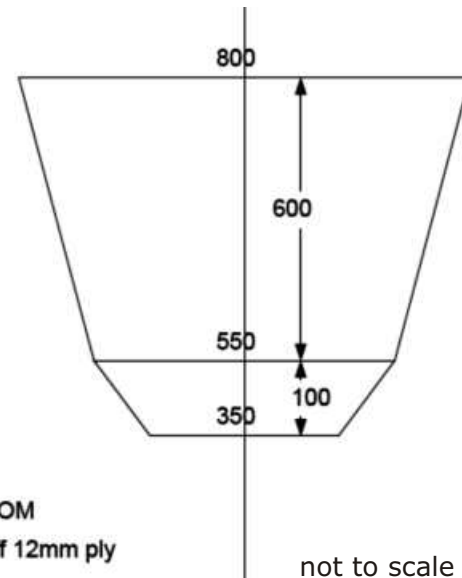
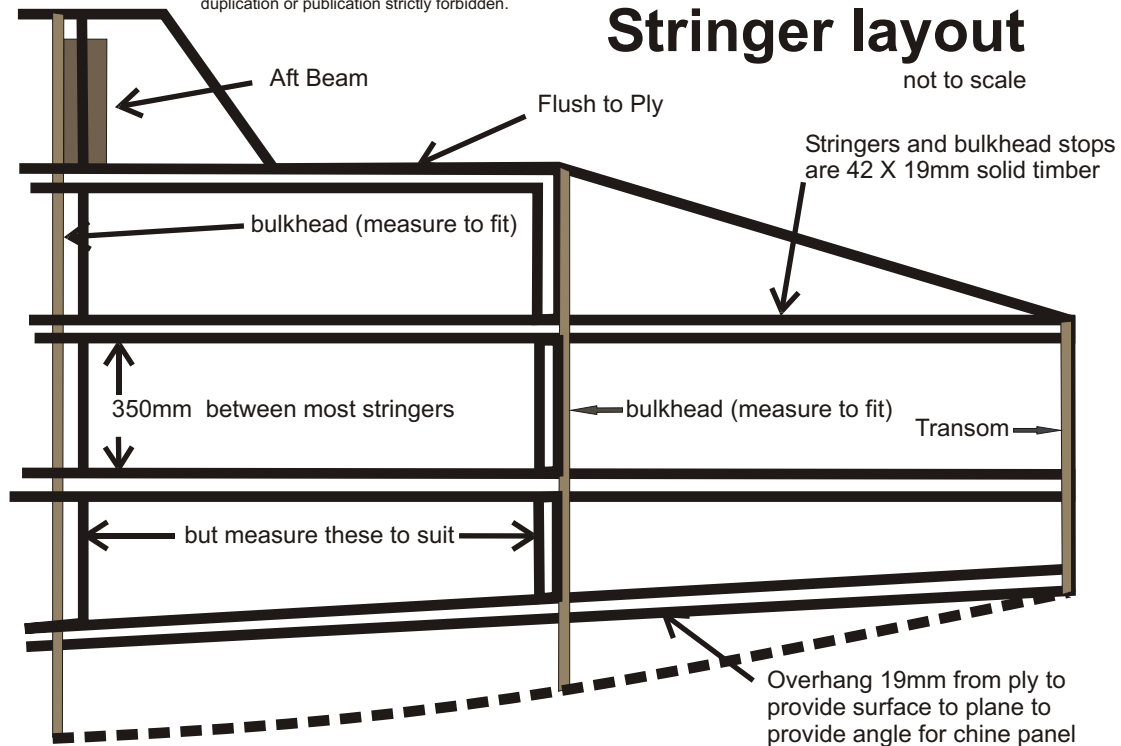
All timber 70x35 pine.

Two (2) laminations of 9mm ply on each side of timber framework and glass whole unit. CTR to CTR of hulls 3600.

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Stringer layout

not to scale

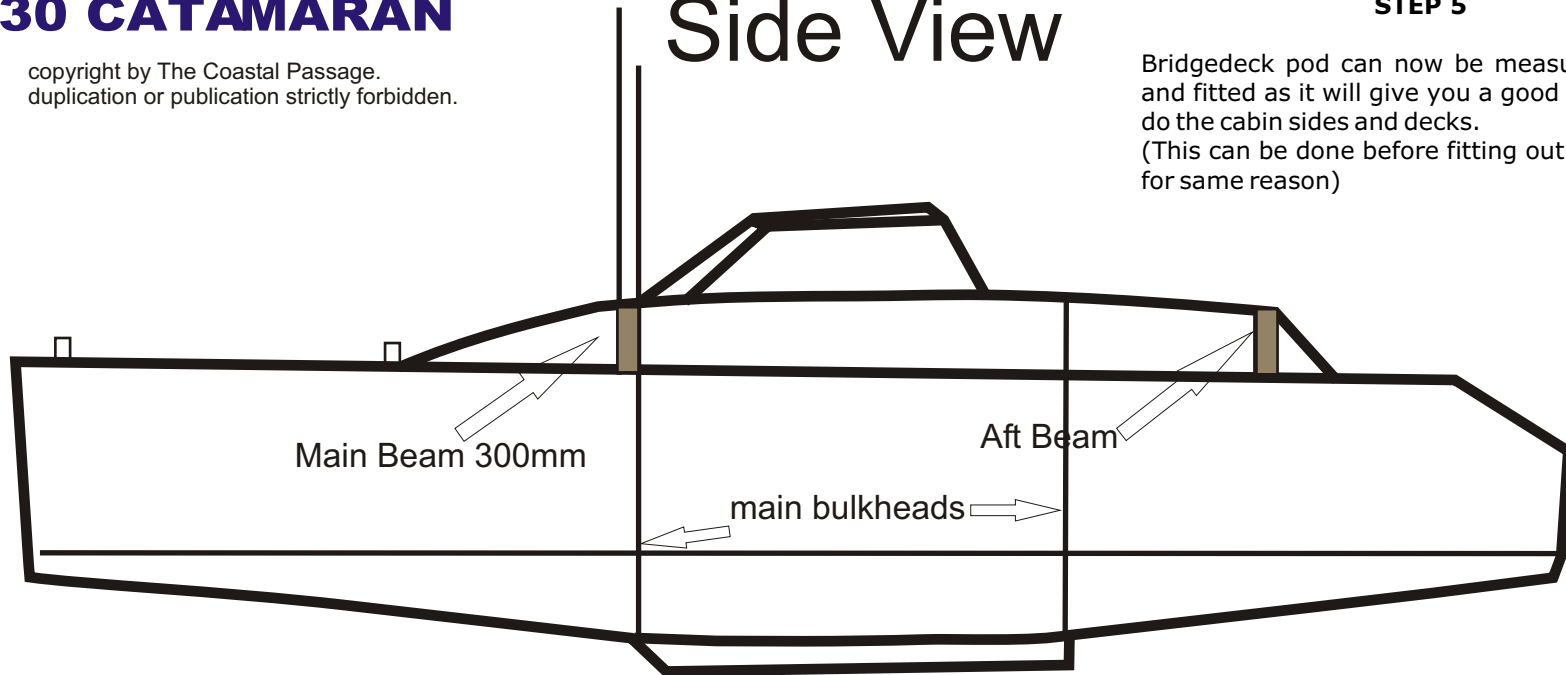


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Side View

STEP 5

Bridgedeck pod can now be measured, made and fitted as it will give you a good platform to do the cabin sides and decks.
(This can be done before fitting out if you wish for same reason)



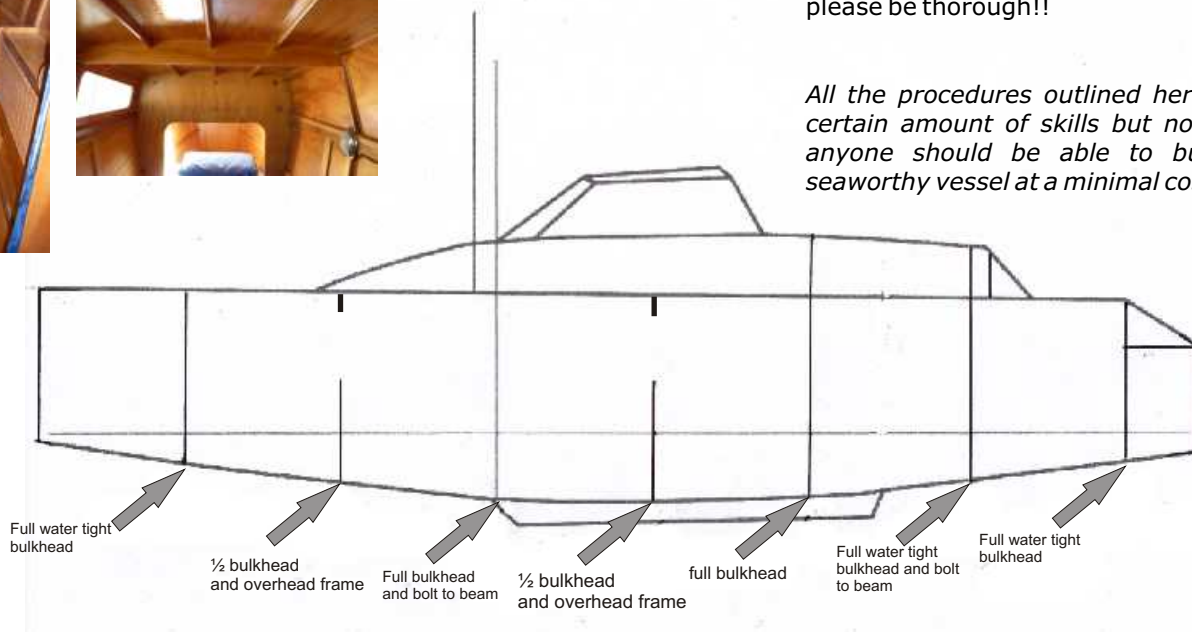
Centre to centre of hulls 3600

NOT TO SCALE



The most important part with any ply boat is the glassing so please be thorough!!

All the procedures outlined here are basic and will require a certain amount of skills but none of it is rocket science and anyone should be able to build themselves a good and seaworthy vessel at a minimal cost in both time and money.



Passage ways cut into bulkheads should leave at least 6 inches/150mm between apertures and hull panels. 9mm ply left over from other sections can be glued/laminated together to form 18mm for 1/2 bulkheads that are cut out for passageways. Everything is optional depending on your fitout but always leave as much bulkhead in place as your plan will allow. Stowage and other furnishings mounted to bulkheads strengthen the boat. The water tight bulkheads make this boat impossible to sink accidentally.

CSC 30 CATAMARAN

CONSTRUCTION TIPS

1I know it is basic but remember to stagger the scarfs on the stringers

2.....Scarf and glue the 4 bottom chine stringers before initial assembly as you can use one as a full length fairing batten for both the main panels and the bottom panel if you assemble one hull first.

3.....The bulkhead stops are nearly all the same length (350mm) so you can set up a stop on the drop saw and cut them all at once(about 50)

4.....If you have cut all the stops you only need to mark the bulkhead positions and not the stringers. Just fit the top stringer flush with the ply then one set of stops and butt the next stringer to them and fix and repeat the process but remember the bottom stops aft of rear main bulkhead will be shorter (measure)

5.....Leave stringers 19mm short of bow to allow for stem timber when you pull everything together.

6.I use construction glue on all my boats as I have found it to be quick strong and readily available(Zero Nails/Liquid Nails) and inexpensive.

I also use a small nail gun and stainless steel nails(quick and easy)

7.....Remember to use the first cut panel in every case as a template for the remaining panels and where applicable to cut pairs of panels if you have a good and bad side to your ply.



CSC 30 CATAMARAN

How to get shapes for intermediate bulkheads or any other shape

Some one named them joggle sticks, good a name as any.

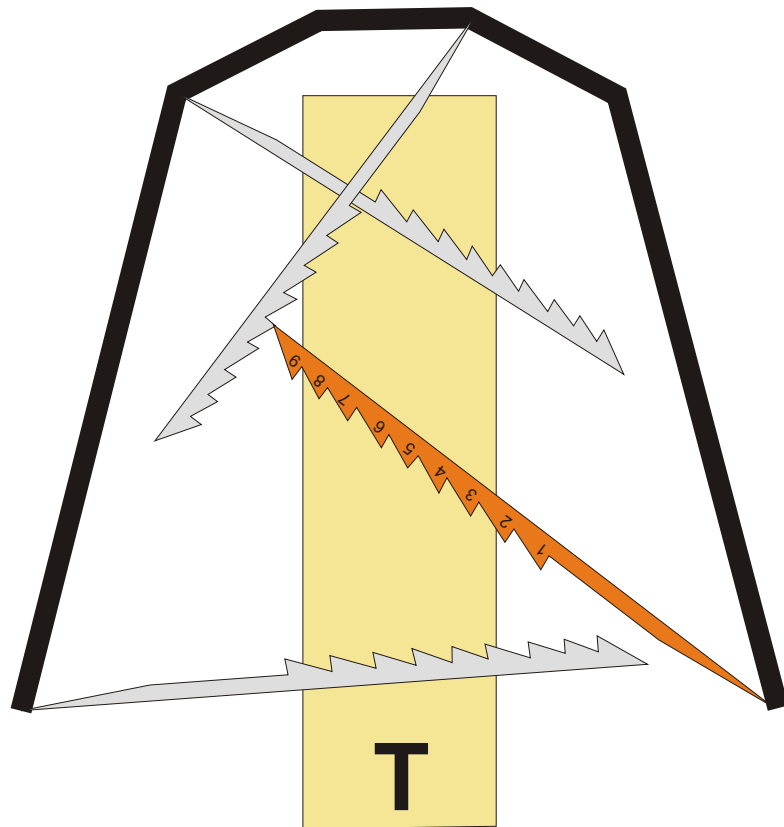
Cut one say, a metre long by 60mm and then cut the teeth and point into it. Tidy doesn't matter too much. Then you need a small piece of scrap wood (T) you can fasten inside say, a hull. It can vary in shape but it has to be secure in place. Then take your joggle stick and put the point in any place you want to make a reference point, any edge or corner.

With a pencil trace the outline of the joggle stick on the piece of scrap wood. You might put on

the numbers you have on the joggle stick too, to help place them later to make your panel.

When you are done with that, remove the scrap wood and lay it on and fasten it down on a panel you want to cut from and place your joggle stick in the places it was traced in and make a mark by the point.

When you have done that you will have a 'connect the dots' game. It is that simple to reproduce those shapes and they will be accurate to a mm if you are reasonably careful.



CSC 30 CATAMARAN

Forebeam and intermediate beam (yellow arrow) are solid timber 150 X 50. Forebeam is about 250mm from the bow end. Use stainless or gal bolts through decks and doubling plates with heavy glass (2 or 3 layers 600 gr), and a coach screw through sheer panel. Don't forget to install pad eyes (red arrows) on reinforced points in line with beam as per photo. Route wire (blue arrows) to centre bolt that also mounts your fore stay.



mast on *True Blue* is 10m with main foot 2.8m and luff 8.25 but you could go to 10.5 or 11m

fully battened and moderate roach.....

CSC 30 CATAMARAN

RUDDER ASSEMBLY CAN BE EITHER
PULL UP OR KICK UP..

SHAPE BLADE TO AIRFOIL SHAPE
AND GLASS AND FINISH

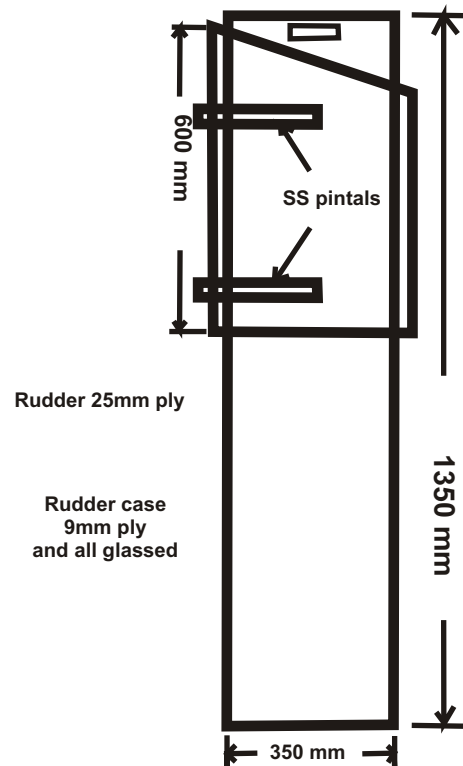
BEFORE MAKING THE CASE TO
ENSURE GOOD FIT AS A LOOSE
FIT WILL VIBRATE .

PINTLES CAN BE FABRICATED FROM
40X4MM SS BAR.....

WHEN SETTING RUDDER DEPTH
ENSURE IT IS NOT BELOW KEEL
LINE....

TILLERS CAN BE TIMBER OR
ALUM.....

Rudder Assembly



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<http://youtu.be/5aP-TMgalrw>

CSC 30 CATAMARAN

suggestions to mount motor..

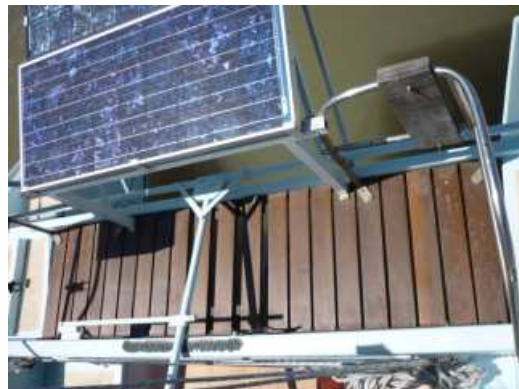


More on rudder box



CSC 30 CATAMARAN

Various useful photos



**Are you over 60?
Poor health??
Heavy smoker???**



**Do you have a pile
of assets and been
agonising over a
will because your
kids are a bunch of
spoiled brats, your
ex got more than
their share already
and the rest of your
rellies are a bunch
of drunken leaches
dropping heavy
hints about what
they want when
you cark it???**

**Do you want
to leave it to
em or stick it
to em?**

Leave it to TCP!!



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bureaucrat or politician.
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devastating impact!**

**Or just because TCP is
free, and is great
reading!**

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Thanks!**



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Mother Nature's Jewellery

Words & photos by Dianne Challis, SC ELAN

One morning I awoke to a fog that rose around the boat from the flat mirror like water.....calm before the storm. It was hard to believe that the day was lulling us into a false sense of security when we knew what was installed for us strong winds and an anchorage that was to become more like the inside of an agitating washing machine.

But when Mother Nature gives you a gift you just run with it. The decks were wet and glistening from heavy condensation. Everything

was either damp or wet. I have been perplexed over a small spider's web that I am constantly brushing away. This eight legged creature is hiding somewhere but eludes my every effort to seek its whereabouts. But this morning I didn't have the heart to destroy all the hard work it had put in during the night. I had before me nature's necklace. The beads of condensation hung like diamonds and reflected light as the sun attempted its feeble attempts of glowing through the fog.

I left that web alone today, I will think about removing it tomorrow.

DIAMOND DECK - endless possibilities

Diamond Deck is a non-slip quality UV stable durable marine grade closed cell EVA foam that will not absorb any water or ever go mouldy.

Manufactured to the highest of marine grade standards Diamond Deck offers the complete protection to gunwales, transoms, tinnie seats, decking, boat bow's, steps, hatch covers and anchor well's.

Diamond deck will also insulate you in winter and reduce heat by 75% in summer making this the perfect all year round Australian weather defence.

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continued next page



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WINDMILLS & WATERWAYS: Cruising the Dutch Canals

Story & photos by Jan Wooller, MY *KUAH*

"It's OK, they've seen us." Before the bridge, a green light joins the red light to indicate that we can keep approaching. Alarm bells ring, boom gates drop and soon the bridge opens allowing our 11.6 metre Dutch canal boat, *KUAH*, to motor serenely through Nick and I waving our thanks to the bridge keeper as we pass.

Despite all our years afloat we were a bit nervous at the start of our first season on the Dutch canals. We had never experienced canal boating before and the idea of tackling very narrow waterways, low bridges, lifting bridges and also transiting locks was a little daunting.

KUAH has a 2.35m air height with her mast, VHF aerial, flag pole and windscreens all folded down, so we approached our first low bridge only 2.60 m off the water - with extreme caution. Soon we became fairly unconcerned about clearances of 2.50 m beneath bridges and have gone under a couple of ones with only 2.40m clearance.



KUAH approaching first windmill for the year

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2.50cm bridge with a 90 degree bend straight afterwards



Another gorgeous canal

It was when the low bridges were only 4.2 metres wide and immediately followed by a 90 degree bend in the narrow canal that it became more exciting. When buying a canal boat we had deliberately chosen one with a very low air height which allows us to negotiate all the tiny, less frequented canals. These quickly became our

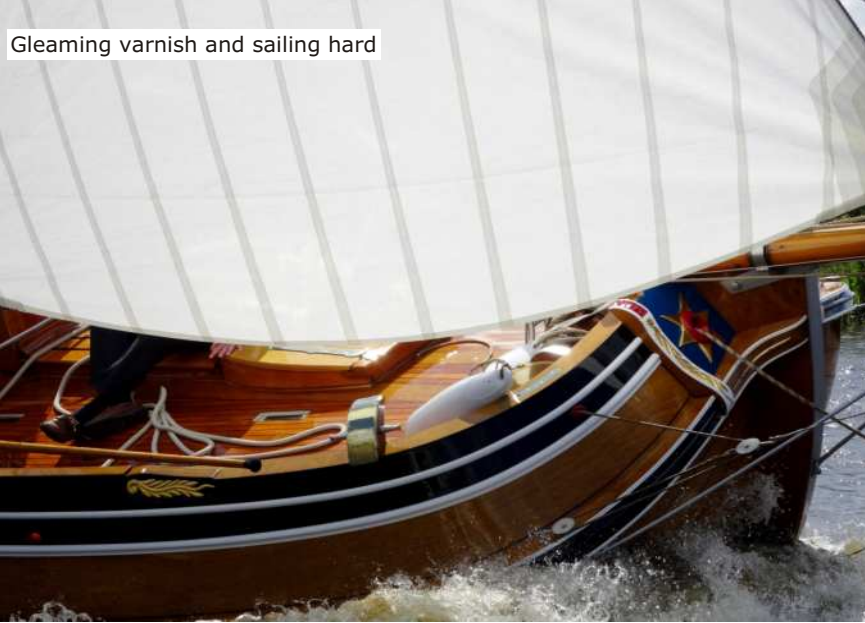
favourites particularly in the height of summer holidays when the bigger canals are busy.

To add to interest, occasionally a bridge toll (usually 2 euro) is required. This is collected in a most charming manner. As *KUAH* motors slowly through, the bridge keeper swings out a gaily painted wooden clog on the end of a fishing pole for us to place our coin inside.

The Netherlands has a huge network of waterways to explore. We meandered along gorgeous wildflower and tree lined canals, past thatched roof cottages and huge farm barns, LOTS of cows and sheep grazing in vivid green fields,

windmills of all shapes and sizes and through delightful historic villages and towns. At times we felt like we were cruising through peoples' back yards as their gardens abutted the canals.

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Gleaming varnish and sailing hard

Every day we gazed in awe of all the gorgeously restored/kept traditional craft (both sail and power) out and about on the waterways. Any one of them would attract admiring crowds in Australia and elsewhere. We can't imagine too many Australians willing to keep all that brightwork gleaming.

To encourage water borne tourism, almost all Dutch towns and villages have docks lining their waterfronts that are available for private canal



Toll bridge. Jan putting coin into the clog

boats to tie alongside. Many have electrical outlets and water outlets and also public toilet and shower facilities. All are free of charge until at least 4pm and often till 6pm, after which, in order to stay the night there is a fee which varied from 4.50 to 18 euro per night. To save money, when possible, we did what the locals do - visited the towns during the day and then motored one or two kilometres to tie up at one of the thousands of free moorings in the countryside for the night.

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Skutsje racing

During our cruise we stumbled across various events including an opera where the paying guests were seated on a barge which was towed into place opposite the canal-side stage, city summer festivals, an Abba tribute concert - this time it was the stage that was on a barge moored mid canal. There was an 11 City race (emulating the famous ice skating race that takes place in Friesland in winters when the ice in the canals is thick enough) which had walkers, cyclists, runners, rowers and even 400



Pat and my sister Pamela watch the 11 city rowing event from their boat *SHANTY*

plus antique cars and more than 200 antique motor bikes following the 110 km route over one weekend, and Skutsje (30 metre, 25 ton traditional engine-less sailing barges) races. It was an amazing sight to see these huge vessels in tacking duels up a narrow canal; one person manning the enormous lee boards, two or three hauling in sheet ropes through ancient wooden blocks, another two wielding the heavy tiller.

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Strontrace entrants



Cycling past fields of flowers

Most days we'd cover only 10 or 12 km along the canals, using our folding bikes to explore further afield via Holland's vast network of well signposted bike paths. With flat land and cool weather even with our 20" wheels and no gears we were soon happily doing loops of 25 - 40 km bike rides most days.

There was always plenty to see - including fields of marijuana (it's legal in The Netherlands so I guess it has to grow somewhere), groups of crazy people who had paid good money to be taken of a guided "tour" of the mudflats of the North Sea, strangely large numbers of mini horses - often penned with miniature goats for company, excellent antique/second hand shops to rummage through etc.

It's a good thing we were doing lots of exercise as we were also making it our business to find the very best Apple Cake in the country - and to eat more than our body weight in delicious Dutch cheese!

continued next page...

Boutange canal. Jan opens the bridge while Nick waits to bring *KUAH* through



Last year, Nick and I took our time in the easternmost canal in the Netherlands - a gorgeous area that is so close to the German border that on one of our bike rides our Dutch phone thought we were in Germany and went onto roaming. Dutch friends we had met told us that the small side canal to Bourtange - an ancient fortified village on the border with Germany - was a *Must Do*.

our antics. All the instructions being in Dutch didn't help me! In reality the drivers were without fail very understanding. It was with a certain amount of pride that I, the newly crowned Bridge & Lock Queen of *KUAH*, got us safely up to Bourtange and back again to the main canal.

We paid our deposit on a very important looking large brass key and tackled the first of 10 self service bridges and 5 self service locks that we would need to negotiate to get to Bourtange. At each bridge/lock, we would tie *KUAH* at the small dock beforehand. Nick would wait with *KUAH* while I operated the bridge/lock then he would motor through and tie up at the dock on the other side to wait for me to close it. Each were different - some had to be pushed, others wound up and down; some were fully manual - others were semi automatic; one was fully automatic (just push a couple of buttons). However at each bridge there was a moment (or longer!) of angst as the delay mechanism was just long enough for me to start to panic that it wasn't going to open/close.... leaving us with irate drivers in the cars stranded by

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One of the big sluys



Boutange Canal
Jan winds up a bridge



This year we've explored different areas and have enjoyed our second season on the Dutch canals just as much as our first. A highlight has been attending Sail Amsterdam, a 5 yearly event involving many of the world's Tall Ships plus many thousands of other traditional craft.

What a spectacle! We were very glad we had heeded our Dutch friend's advice and left *KUAH* moored about 20 miles outside Amsterdam while we caught the train in. They had told us we'd be "risking life and boat" to have *KUAH* in among the mayhem and having seen it, I'm sure they weren't exaggerating.

The Sail Past of all the vessels arrived in downtown Amsterdam at about 2pm. Barely a metre of water appeared to separate the boats that packed the huge canal for as far as the eye could see. Four hours later boats were still arriving in the same numbers. Surely the Netherlands is the only country in the world with enough dedicated owners of traditional craft to be able to stage such an event. Put it on your calendar for 2020!

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The Coastal Passage 75, November - December, 2015



A colourful fishing village

Colombian sailors put on a show at Sail Amsterdam,



Blue board displayed on ship

KUAH has also followed the 14th and 15th Century trading route down the Ijssel River, visiting the historic Hanseatic towns of Doesburg, Deventer, Hassem, Zwolle and Hasselt each of them boasting wonderful old buildings, huge churches with tall towers and some of the thick walls that provided fortification hundreds of years ago.

As we swooshed downstream with 2 to 3 knots of current behind us, we were passed by the modern day equivalent of ancient sailing barges - huge motorized barges mostly loaded with gravel or sand.

Many showed a Blue Board on the bridgedeck indicating that they wanted us to pass Starboard to Starboard in the narrow river. All had permanent living quarters for the captain's family plus a modern car on the stern.

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As I write this, *KUAH* is moored alongside a free dock in the countryside. Tall reeds that are harvested for use as roof thatching sway in the wind along the opposite canal bank; black and white Friesian cows graze on lush green grass in the fields beyond us on "our" side. Flocks of noisy geese fly overhead. Occasionally cyclists ride past on the Fietspad (bike path) that runs nearby.

Nick is doing a bit of routine maintenance prior to leaving our boat for the winter. Tomorrow morning while we wait for the overnight dew to dry from the decks we'll head off on the bike path too. There's yet another gorgeous looking village only a few kilometres away that caught our eye on our way past that we want to explore.

By the end of October we'll be back in Tasmania, our second season on the Dutch canals at an end. Although most people think of France when considering canal boat cruising, we're in no rush to leave the Netherlands. Incredibly for such a tiny country, it has so much more for us to see and do and we'll look forward to coming back again next year.

SHANTY and *KUAH* tied next to the windmill at Sloten. Laundry day



WINDMILLS & WATERWAYS: more photos from Jan



Ex working farm barges race down a canal



Church wind vane shows the sailing heritage of the area



One of several Sea Shanty groups we came across

KUAH taking on water at Sneek



A misty morning

Overtaken by a 3 masted sailing vessel



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Passage Planning From the SW Pacific to NZ.

**By John Martin, Principal,
Island Cruising Association**

With the 2015 Tropical Cruising season coming to an end, many skippers are starting to look at the weather and what's likely for the passage back to NZ. With today's technology weather information is more readily available and the accuracy has improved immeasurably over the last ten years.

Weather routing packages like PredictWind's Offshore App have also made daily routing while on passage a reality, taking the guess work out of the myriad information sources and displaying it in an easy to follow logical presentation of the data. I wouldn't be without it. While it's rapidly becoming unnecessary to have an in depth knowledge of weather and how it happens, good seamanship still dictates that while we may be able to get cutting edge information any time we want it we still need to have a basic understanding of the mechanics.

Experience is also a wonderful thing sometimes hard to get but easy to pass along. That's where this article leads us.

There are a lot of myths surrounding the passage to New Zealand from the Pacific islands. Most are the result of inexperience and lack of knowledge. I've done the trip personally now 42 times, soon to be 43 with our upcoming passage back from New Cal to Opua and only had one really bad experience, the very first trip. I decided after that first trip there had to be a better way so I started gathering some information.

The first thing I learned is don't just pick a date and go. As it turned out boats that left just 3 days after we did, had a great trip. Weather is the governing factor when making this passage and your strategy will vary depending on your departure point. What doesn't change is the nature of weather in this region. In the South West Pacific, the weather systems

migrate from west to east. Low follows high follows low in a never ending succession.

Coming across the Pacific from French Polynesia the trade winds blow south east and east with the occasional hiccup to the north and north west as a front or trough passes through. These troughs (unless you are being affected by the SPCZ) are usually associated with a low pressure passing to the south. The strength of the trades at the STR (sub tropical ridge) is dictated by the center pressure of the high pressures passing below and the latitude of its center. These are the generalities and I will give you some events to look out for that have an adverse influence on weather for a passage at the end of this article.

As the high pressures migrate east, the direction of the winds change. In the trade wind belt this variation is less pronounced but as you travel further south and move away from the influence of the STR the winds will tend to follow the rotation of the systems. Both highs and lows. Highs rotating in an anticlockwise direction and lows, clockwise in the southern hemisphere.

continued next page.....

Passage Planning

The effect a high pressure system has on the trades depends on two factors: the center pressure and the latitude of the center. A 1020 center pressure high will give 15 to 20 knot trades at the STR (where the high meets the inter tropical low). As the center pressure increases so do the winds along its top edge. This area of wind, the STR, is the squash zone between the high pressure cell and the inter tropical low (which is the general area of low pressure either side of the equator). The latitude of this squash zone is dictated by the latitude of the center. For example, a high of 1030 with a center at 30 south will give significant winds (30 to 35 knots) from 15s to 25s with the wind decreasing the further south you travel. This same high, but centered over the middle of New Zealand, will have lower winds on its top edge (20 to 25knots) but spread over a larger belt. This high is also likely to have a larger area of little to no wind in its center. The high pressure centers tend to trend south as summer starts in NZ. As you travel further south the effect and intensity of the frontal bands associated with each low are magnified. The majority of low pressure cells during October and November

migrate along under Australia and tend to pop north east in the Tasman, cross NZ and migrate to the east. All low pressure cells will have a frontal band originating well north of the system and rotating around the eastern side of the low, to its center. The closer to the low and the deeper the low's center pressure will dictate how much punch it has. As the front moves through, the wind direction will change (as much as 90 deg) usually to the west and southwest with a disturbed flow. Rain is usually associated with a front with clearing showers behind. The strength of the wind in this area is determined by the pressure gradient. There will also be large variations in wind strength around rain cells.

So to summarise: the more intense the high the more wind you will get on its edge and the punch from a frontal band is greater the closer you get to the low. We also know the wind direction around a high is going to change as the high moves through and this is more pronounced the further south we are. We know further that as the high migrates through it will be followed by a low and its associated frontal band. In the weeks leading up to your passage we need to be looking at the speed that the systems are coming through. This will be between 5 and 8 days typically. An 8-day cycle

is easier to plan for, but short is OK too you just take a different approach. Each year is a little different, this year, 2015, has seen a little more activity in the weather, with the systems coming through faster and cell activity both off the Southern Solomons. and off the east coast of Australia. The high pressures have been moderate in their intensity and small in size. The center of the highs are still coming through north of NZ and should start to drift further south over October, easing the squash at the STR. Long range forecasting is predicting the center pressures to remain under 1030 so it will be interesting to see if the overall size of the systems gets bigger and the length of time between systems slows down. As for the lows, New Zealand has seen some deep low pressure systems over winter which has been wet and cold, not unusual. Indications and reports from those in NZ would indicate that Spring is there and the weather is warming and the intensity of the weather systems is moderating. So keep an eye on how the patterns develop in October and if you see a good weather pattern developing, take it. It's better to arrive in NZ a little early. No problem there's plenty to do there. OK, so now we need to consider where we are departing from.

continued next page...

Passage Planning

Departing from Tonga

Tonga, the most eastern departure point has some advantages and some disadvantages. From here we are looking to leave in the top of a high pressure as the wind swings from south east towards the east.

But being so far east means that once the wind is in the east the high pressure below is also well east, you will only have its influence for a short time. The advantage is that you can use Minerva reefs as a stop off point if required and sit for the next window. From Minerva you are only 800 miles to NZ at an average of 5.5 knots or 130 miles per day this will take 5 to 6 days.

Again it's likely you won't be able to do this in one weather system so you need to plan to have a frontal system somewhere along the way. This is going to have less punch if you are still north of 30 degrees south and with the wind direction changing to west

It's this decision to take the shortcut that causes most of the horror stories you will hear about.

and southwest behind the front the plan is to have put in enough westing so that you have cracked sheet for the last leg into NZ. We usually plan to aim for a spot directly north of North Cape, New Zealand's most northerly point, at 30 south. This means quite a dog-leg from Tonga but is well worth the effort.

If things change as you head south, then the strategy can be changed. If you do decide that the high is moving slowly enough that you change your mind and head straight for NZ, make sure you can get there before the front comes through because those lovely NW winds you are enjoying will be right on the nose after the front passes. You are also likely to get an uncomfortable 18 hours as the front passes.

It's this decision to take the shortcut that causes most of the horror stories you will hear about.

So, in a nut shell, leaving Tonga. If you plan to stop at either North or South Minerva (North is a more secure anchorage) then wait until a high pressure (of around 1020 to 1025 center pressure) is giving an easterly flow. You should be 2 to 3 days to Minerva with the wind tending northerly when you get there. Because you will be looking to put some westing in, on the next leg, you can leave from there on the front of the next high when the wind is still in the southeast. Depending on your progress and how fast the high is moving will dictate your strategy as you progress south.

Remember it's better to take a front at 30 south even if you have to heave-to for a day and wait for it, better still use the time to get further west. **Remember the old expression, west is best.**

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Passage Planning

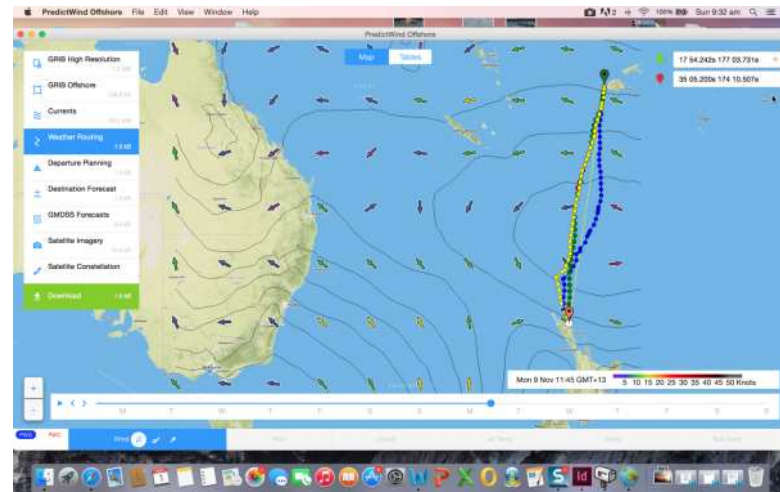
Departing from Fiji

Fiji on the other hand doesn't have the advantage of a stop on the way and is the longest of the passages at 1260 miles, so you will need to leave a little earlier as the high starts to fill in; you are still looking for a high of around 1020 to 1025 centre pressure. This will undoubtedly mean leaving Fiji in a little more wind than most would like but again you will want to get westing in from here too, so ease those sheets and go for it. There is quite likely to be a squash zone along the coast of Viti Levu if you are leaving from the Lautoka area, this usually starts to drop 12 to 18 hours out. By leaving on the front of the system you are also more likely to run through the middle of the high as you progress south

where winds will be light. Your strategy forward from here will be dictated by your use of the motor. If you have plenty of diesel and don't mind the donkey clattering away put peddle to the metal and start heading for the same point as described above, 30 south directly north of North cape. If the high is slow and you will make it in before the next front then great, go for it. You should get strengthening northerlies and northwesterly winds on the back of the high. Remember when

it swings to NW the front is imminent. Regardless of how you think you are doing, don't vary your approach too much. If you want to cut the corner and you feel you have the time, change your waypoint from the one above but make North Cape your next waypoint. The fronts have a tendency to jump up from down south very quickly, if you are hugging the Northland coast by then you will be in relatively protected waters.

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Passage Planning

Departing from Vanuatu and New Caledonia

Vanuatu and New Caledonia departures can be regarded as having the same strategy. Leaving from Vila, unless you wait for easterlies (when the high is directly below) you will be hard pressed to get past the Isle of Pines on port tack. From there you are looking to head for Norfolk Island. There is no need to be hard on here as the high will go through and give NE, North and then NW before the next system/front comes through. We have waited at Norfolk for the systems to pass and reached into NZ on west and SW winds, on the beam.

Tips and Tricks Things to look out for:

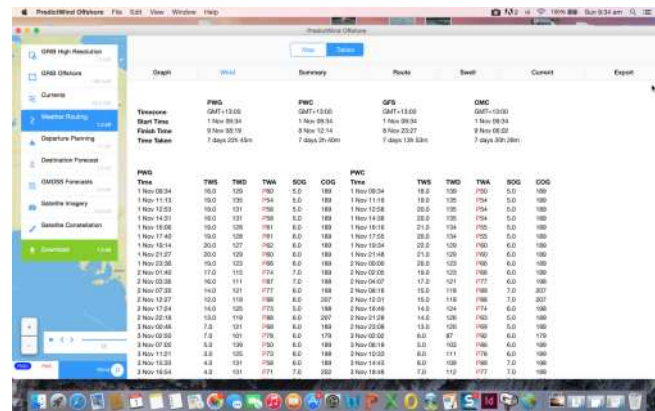
If you are leaving later than early November keep a good lookout for anything with a closed isobar in the tropic region to your west, if you see one DON'T leave until it passes or disappears. As you get later the likelihood of low pressure cells forming in the Coral Sea (and particularly just south of New Cal) increases.

If there is any chance of one of these lows forming on your passage don't leave.

General strategies:

Look for a high of between 1020 and 1025, get west, take fronts north of 30 south, motor if your speed drops and don't cut the corner unless you are sure of getting in before the next front. The only other advice I can give you is to pick just one source for your weather advice. If you allow yourself to get into groups that are discussing weather and they are using different sources you're in

trouble we call this analysis paralysis. You may never leave! Get the right info. We use and recommend PredictWind for weather information, there is a 20% discount link on the ICA home page for this service.



About the Island Cruising Association

Their motto is "We make Cruising more Fun" but there's a serious side too. ICA offers an ever-growing knowledge base of cruising resources and information specific to Extended Coastal (New Zealand and a developing section on Australia) and Offshore, with an emphasis on the South West Pacific. They offer a wide range of fun events, cruising rallies, training, practical demonstrations, on the water preparation and backup to assist cruisers to get out there. Upcoming events include the FREE "All Points Rally" in November 2015 (from all major SW Pacific Ports into Opua, NZ); the CPS (Cruising Prep Seminar) in Auckland in November this year and Brisbane and Cairns in June/July 2016; Multihull Solutions Sail2Indonesia Rally and Doyle South Pacific Rally in 2016; and the Doyle Pacific Circuit Rally on again in 2017. It's all happening!

Membership in the Island Cruising Association is NZ\$65.00 per year and membership gives you access to the entire knowledge base.

www.islandcruising.co.nz

How to write a killer headline to sell your boat

By Stuart Mears, SY Velella
and Author of "*Over Boated?*"

There are two fundamentals in advertising a boat for sale. The first is to be clear that you're advertising a destination not the airplane. Confusion on this point is the first man-hole trap that many vendors step into.

It's perfectly obvious that no-one in their right mind these days, travels for the experience of cramped airplane seats, muscle mangling leg-room, ghastly food and the horrors of jet-lag. If the travel experience is worth it at all, it's due to the pleasures of the destination. However it is easy to forget that this principle applies equally to copywriting, particularly in the emotional turmoil that often accompanies the selling of a boat.

The second vendor man-hole, no less critical to buyer response is the need for clarity about 'the problem'. What you might say has 'a problem' to do with writing a killer headline? Clarity on the distinction between *your problem* as vendor and the *buyer's problem* is critical to the success of your boat for sale headline; as it is indeed to the entire exercise of marketing your boat. The thing is, in the business of buying and selling, everyone has what might loosely be called for want of a better description, 'a problem'. If you're unlucky enough to be a desperate boat vendor, struggling to stave off bankruptcy, your problem is immediate and in your face.

But so too does the boat buyer, have a problem. Sure, it's more subtle; not the great stinking wildebeest of a problem that confronts the cash-strapped boat vendor. Indeed the buyer's problem might better be described as a 'need' or 'itch to be', 'to experience' or 'to manifest' something ill-defined within the buyer's psyche. But the urge is just as real in driving the impulse to action. The point is that as a vendor advertising *a boat for sale*, the man-hole trap that many vendors unconsciously step into, is that of writing a headline that reflects *their problem* not the *buyer's problem*.

It is important not to make this mistake because such a 'back-the-front' headline never succeeds! A boat for sale headline that describes the vendor's problem is back-the-front. The fact is that no-one on God's green earth is interested in your problem as vendor, however urgent and pressing it may be to you.

In fact the odds are pretty strong that your problem, if it is properly teased out, is actually 'buyer repellent'. That is to say; proceed deep enough down that rabbit hole and your erstwhile buyer will be wanting to jump out of the first available window to escape your proximity.

For these reasons the first step is always to develop a mental picture of the 'probable buyer'.

to be continued.....

STUART MEARS

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“Let’s go buy a boat!”

How an old wooden 25-foot gaffer found a new home

A TCP “Classic”

By Catherine Connolly, SY, Madrona

It was one of those days in Washington's San Juan Islands, early January 2002. One of those cheerful Seattle gray-sky days, one of those air-eerie-with-mist days, one of those not-quite-cold-enough-for-a-winter-coat days.

“Let's go buy a boat,” I said with a mixture of seriousness and a “Let's see what he'll say to that” playfulness. The New Year's celebrations were over because we had run out of money to celebrate with, and we needed something to do that day. Let's see what he'll say to that.

I had hardly any sailing experience, didn't know the difference between a traveller and a Bimini, didn't have any money (having spent it all celebrating), and had been dating Aaron for less than six months.

Aaron had recently sailed from Beaufort, North Carolina, to Stavanger, Norway, via the Azores,

Ireland, and the lochs of Scotland on a 1935 wooden Norwegian trawler that had been converted into a ketch, sans keel. Aaron had built boats, lived on boats, actually knew how to sail a boat. Let's see what he'll say to that. “What an excellent idea” is what Aaron said. “What an excellent idea. Let's go.”

Suddenly we were in the car driving to a marina to “buy a boat.” Aaron asked me what our boat would look like. “Well,” I stammered because the whole situation I now found myself in was based on what Aaron would say about buying a boat, not what Aaron would do about buying a boat. “She'll be green, probably about 28 feet long. I think a wooden boat would be nice.”

That's how much I knew about boats: I wanted a wooden one.

continued next page...

Catherine sailing on *Madrona*



Tree climber

Since we had only been dating for six months, we still enjoyed talking with each other and spun ourselves a conversation about trees. Aaron, a professional tree climber, told me his favourite tree was the madrona. Indigenous to the Pacific Northwest, madrona trees are curvy like live oaks, have red bark, and like to grow on rocky outcrops.

We pulled into the marina and there she was. A 25-foot, wooden, gaff-rigged Seabird cutter with a green hull and a "For Sale" sign. Her rudder post was the perfect likeness of a winking owl carved in yew; her rudder was the size of a small pony. The tiller was a sexy curve of cherry, and solid spruce spars graced her white decks.

Down below she resembled a hobbit hole, complete with a tiny wood-burning stove and a gigantic 25-hp Atomic 4 engine. She had been well built and well looked after. Her name was *Madrona*.

The owner met us at the marina that January afternoon. After rebuilding *Madrona* in 1988, he cruised the San Juan Islands, teaching his children how to sail. Now his children were grown, and he'd moved on to the world of fiberglass and bigger boats. Arrangements were made to launch *Madrona* the next morning.

Splash! After being in dry-dock for 18 months, *Madrona* swelled up in a matter of hours, taking on a few gallons of water and then nothing. She floats, but does she sail? The wind was blowing about 15 knots, just about perfect for that clunky gaffer. The sun was shining. We motored out of the marina, raised the sails, and off we went.

I didn't know much about sailing that day. Aaron will argue that I still don't know much about sailing, but after two-and-a-half years together, I rarely listen to him anymore. But I do know how that little wooden boat felt the first time we sailed her, how she felt happy, how solid and content she was. How sailing her felt holy. Aaron and I made an offer soon after that glorious sail.

Sometimes I wonder if we found *Madrona* or if she found us. It was as if that little wooden boat, both serious and playful, became an animate object, negotiating the whole deal between us three mere mortals.

*Bob's note: This story was originally printed in **Good Old Boat**, an American sailing magazine that Alan Lucas turned me onto that I enjoy very much. Well worth a look if you can get hold of a copy or to subscribe, as I do. www.goodoldboat.com*



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Spoils of the Sea

A TCP "Classic"

Story & photo by Stuart Buchanan, SY *Pluto*

Devoted beachcombers who were around prior to the 1990s would no doubt agree that one of the greatest finds was a Japanese glass float. These fascinating glass balls, usually about 300mm in diameter and covered in hand-plaited tarred rope mesh, came in a dozen shades of green, blue and amber.

They were used by Japanese tuna fishermen to support their long-lines, which were many kilometres long. Every now and then a float would break away from the line and begin its journey over thousands of miles of ocean at the whim of wind and current, collecting barnacles and other marine life until it was eventually cast ashore.

I was envious of people who had glass floats hanging from the beams of their patios and verandahs, flaunting their prizes as if proclaiming to the world their affinity with the sea.

I found my first glass float in 1966 on North Stradbroke Island. I was standing on Point Lookout headland scanning the main beach with binoculars when I sighted a black spot on the water's edge about 4 kilometres away. It had to be a glass float! But alas, there was a fisherman walking along the beach, halfway between me and the black spot. I took off like a rocket, down the headland and along the beach; it was the only time in my life I've ever run 4 kilometres nonstop. The fisherman got the shock of his life as I panted past him to claim my prize. It was a glass float, and to this day it takes pride of place in our home.

In the early 1970s my wife Shirley and I flew by light plane to Orchid Beach on the ocean side of Fraser Island. We planned to walk 40 kilometres along the beach to Sandy Cape lighthouse. Two weeks earlier a cyclone had washed away thousands of tonnes of sand from the frontal dunes, leaving a 4 metre sheer drop to the beach and exposing rocks that prevented vehicle access.

continued next page...



Twenty kilometres from our destination we came to a wash-in where the cyclonic seas had deposited a mountain of flotsam. We couldn't believe our eyes; within one small area there were twelve glass floats. It was as exciting as winning Gold Lotto. But our joy was short-lived; how the hell could we carry twelve glass floats for the next 20 kilometres?

With two floats in each hand, I set off along the beach. Each float weighed about 5 kilograms and together with my 20 kilogram backpack and the energy-draining summer sun, after a few kilometres, I had to abandon two of them. Shirley had selected the best of the twelve and, despite the weight of her backpack, clung to her prize like a limpet for the rest of the trip.

And then, a short while later, we came across the ultimate beachcombing find - a Japanese lightbuoy. This elaborate piece of equipment was framed out of brass tubing and stood 1.5 metres high. At its base was a watertight cylindrical bowl containing a 6 volt battery that powered a light encased in a glass prism fitted to the very top of the frame. A photoelectric cell activated the light during the hours of darkness. In the centre of the frame, roped between its two curved arms was a large glass float. This type of buoy was used by the Japanese tuna fishermen to locate the end of their line at night. It was an extremely rare find.

As it weighed nearly 20 kilograms, I had to abandon my two glass floats and struggle on with this awkward, smelly, barnacle-encrusted monstrosity. But after 4 kilometres I knew I couldn't carry it the remaining distance. In disgust, I dropped the buoy on the beach and walked on empty-handed.

Next day when we reached the manned lighthouse at Sandy Cape, we told the Head Lightkeeper Harry Tate about our finds. Harry told us he was an avid collector of glass floats.

"Gee," he said, "it's a pity the cyclone has exposed the rocks on the beach. We could have driven down in the Land Rover and got the lot."

"Well," I replied, "you can't drive to the area where we found the floats from the Orchid Beach end, but from this end there's only one set of rocks that I think we could negotiate at low tide."

"It's low tide now!" Harry exclaimed, as he leapt from his chair and headed for the Land Rover.

Two hours later we were back at the lightstation with all the floats, including the lightbuoy. Shirley kept her float and I kept the lightbuoy, while we insisted that Harry took the remaining eleven floats.

The only other lightbuoy I found was while sailing in the 1980 Brisbane to Gladstone Yacht Race in the 11 metre sloop *Aeolus*. We were abeam of Fraser Island when one of the crew sighted a lightbuoy in the water. We went about, sailed back a few hundred metres and picked it up. Ray the skipper, who had been having a nap below, heard the commotion and came on deck. On discovering we were sailing south instead of north, he went off his head; but when we got it aboard, even he had to admit it was a beauty. I can't imagine the skipper of *Wild Oats* being so forgiving.

During the 1970s Shirley and I walked hundreds of kilometres along isolated beaches and around islands searching for these enticing glass baubles. We swam across creeks, prowled through sandfly-ridden mangrove swamps and descended almost sheer cliffs just in the hope of finding another glass float.

And there were other ways of getting them. One day, while we were living on Booby Island in Torres Strait, the pearling lugger *Ruby Charlotte* anchored in the lee of the island.

continued next page...

Spoils of the Sea

Attached to the lugger's rigging were dozens of plastic floats; amongst them was one glass float. I went out in the dinghy and offered the skipper three polystyrene floats in exchange for the glass one. He readily accepted.

One evening in the early 1980s while I was at a Brisbane wharf, I noticed a Japanese tuna fishing boat tied up nearby. On her deck were hundreds of glass floats. I spoke to one of the crew, but he couldn't understand English. Not to be beaten, I drew a sketch of a float on a piece of paper and offered him \$10. He laughed, went on board and returned with four floats, refusing to take any money.

By the mid 1980s glass floats were quickly being replaced by plastic floats. While living on Pine Islet as lightkeepers in 1981, we experienced a bout of unusually fine weather. There wasn't a breath of breeze, only a long, low ocean swell. Another lightkeeper and I took our 4 metre dinghies over to the high, almost sheer cliffs on the southern side of South Percy Island. With only a few small pebbly beaches, most of the shoreline consisted of piles of large boulders making it impossible to land. We anchored about 100 metres out from the cliffs and swam ashore with a long length of rope. We found dozens of floats, which we tied into a raft and floated them back to the dinghies. We must have swum ashore at a dozen different places. By the end of the day we had over eighty floats – only a few were glass.

The last glass float we found was in 1984 on Rodds Peninsula north of Pancake Creek. But you know, glass floats ain't glass floats. There are roller floats, cylindrical floats, herring net floats, floats with spindles, double ball floats; blue, green, amber, lavender and pink floats. Every glass float was hand blown and sealed with a glass plug, sometimes stamped with the Japanese manufacturers' symbol and sometimes with a serrated edge.

A book, *Beachcombing for Japanese Glass Floats* was published in America in the 1970s and describes everything you ever wanted to know about glass floats. It sets out their varying sizes and the rarity percentage of finds.

Not everyone is fanatical about finding glass floats. I met a professional fisherman in Gladstone who said if he came across a glass float while walking along the beach, he'd kick it out of his way.

There are many yachties, most of whom are now in their sixties or seventies, who spent weeks scouring our beautiful coastline and islands searching for and finding these fascinating baubles. I know yachties who would never part with their collection at any price, even though glass floats have now become quite valuable.

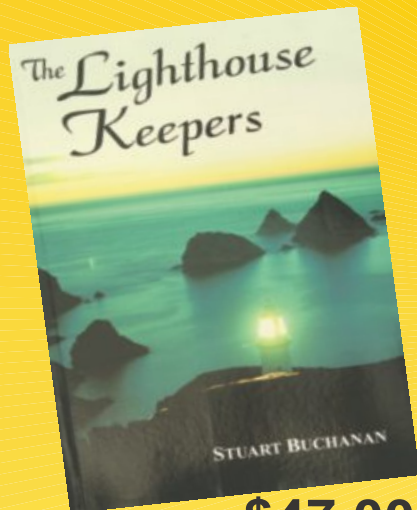
I don't really know why glass floats are so fascinating. We've got them hanging from beams in our house, on the verandah and around the garden. They collect dust, spiders' webs and bird poo. But there's just something about them.



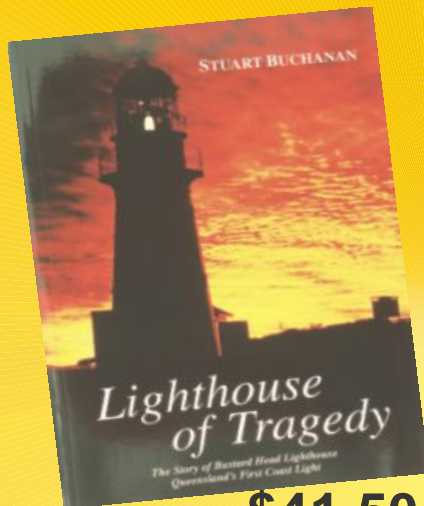
Stuart

When I throw off this mortal coil, what's going to happen to them? They'll probably be sold off for a pittance at some auction along with our other pathetic possessions. Perhaps I'll stipulate in my will that when my ashes are chucked into the sea, the glass floats can be thrown in after me to once again wash ashore on a beach to be rediscovered by modern-day yachties. And if you don't find them fascinating, too bad, just kick them out of the way.

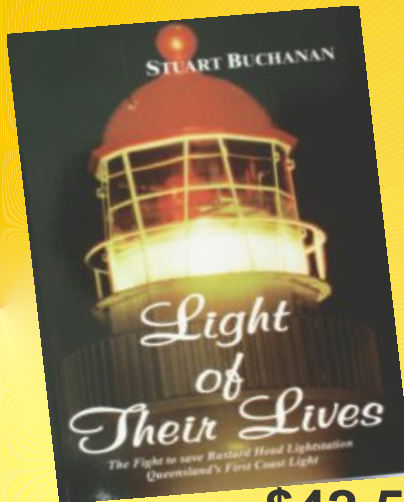
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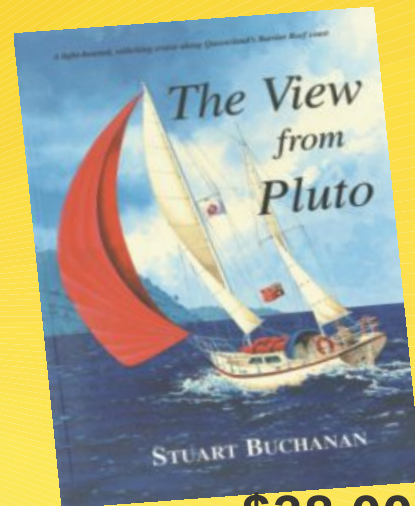
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Turtles at Lady Musgrave Island

By David Hawkins, SC Quoll II

At night time on Christmas Eve 2003 we walked around the other side of Lady Musgrave Island to the guide who was going to show us the turtles laying their eggs. We watched one female turtle dig her nest. She put her back flippers down and threw the sand and coral out of the hole. Her back flippers kept on digging on the sand like a bucket. It is a good idea to stand on the side because the coral can come at you like a rocket.

When she stopped digging, she started laying her eggs. We watched her and were allowed to take an egg out. It felt in between hard and soft. When you pushed down a bit not too hard, it would pop up. We didn't get to watch the hatching that season but we did this year.

Just before Easter, we stopped at Lady Musgrave

again. At night, we went on shore and found a baby turtle going down to the sea. We helped it a bit by shining a torch in front of the young hatchling.

There were some seagulls around but they were scared of us. We heard them squawking because they wanted some more food.

Lady Musgrave Island is a great place to swim, snorkel and walk. Best of all was watching the turtles lay. I look forward to watching them again sometime.



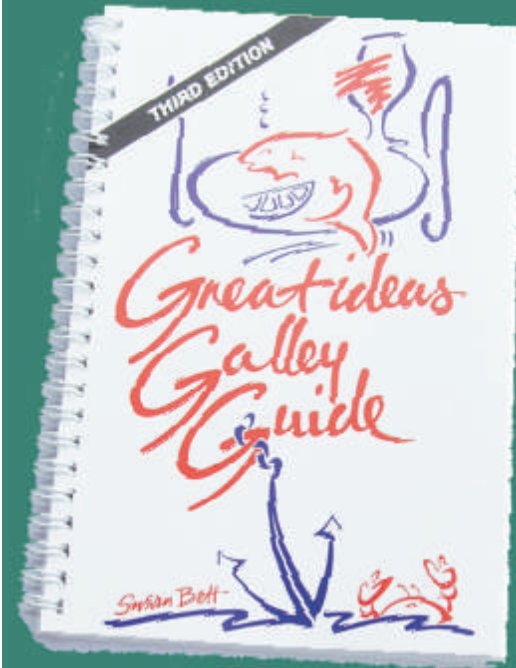
David & little brother Matthew get in close



*Lot's of eggs,
& a young one on its journey*

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Discovering our good mate Captain Cook

By Keith Owen, SY Speranza

Yes, we did Cook as a history project while still at school. But now having sailed through waters charted by Captain James Cook RN (ret) on the barque *Endeavour*, we wanted to know more about his trip. And we were surprised in what we learned after some further reading.

Of course, Jim's main purpose was to take some scientists to Tahiti to observe the passing of Venus and the Sun. He was then invited to open a sealed envelope which contained instructions that, if he still had enough time and inclination, he should go and look for the mythical "Great Southern Land".

Well Tahiti was a ball. Jim and crew had a lovely time "fraternising" with the locals. The women were very willing to exchange their favours in return for ship's nails (of all things). Gives life to a great line "Hey, how about a screw for a nail?" So much fun was had that the *Endeavour* nearly collapsed because the crew had flogged too many of the ship's fastenings as "currency" to pay for their disportment on shore.

Then the big Venus gig occurred and what happened? They stuffed up the experiment. Jim had embarked with quite a bit of grog on board - 1,200 gallons beer; 1,600 gallons brandy/rum; 3,032 gallons wine. The whole crew were obviously all very pissed at the actual time of the solar event and their attention was directed towards the wrong Venus. Their measurements didn't add up. What a circus. Bloody long way to go for a nil return!



The re-enactment of Captain James Cook's landing at Cooktown in 1770, is held every year at Queens Birthday June 8th) weekend during the Cooktown Discovery Festival.

photo by Marlene Leith

So Jim, fearing that all the cavorting in Tahiti (which bugged up the Venus experiment) would not look too good on his cv, opened the sealed envelope and headed off south to look for Oz. But he found New Zealand first. He loved NZ. Not as much sex available as in Tahiti, but he enjoyed the food and made friends with the Maoris. Jim reconned NZ was worth the trip. (Rumour has it that Jim is still the number one ticket holder of the All Blacks footy side.)

continued next page...

Then sailing west to Oz, Jim arrived at Botany Bay (near the airport). Some natives approached him. He tried to communicate and left some trinkets on the ground as presents. Were the astute aborigines going to lured by a few tacky throwaways from the local corner Marks and Spencers store in distant Whitby UK.? Not on your nelly - they turned their backs and walked off (as one would). Jim was devastated that he had come all that way just to be rebuffed by some bloody Aussies so he got the shits and said he was going home (true that Tahiti had made a bit of a dent in his grog supply and he needed to top up).

So he sailed north. He had a copy of Debrett's Peerage under his wing and every bump in the coastline he passed, he called it after a mate in high places. For example, Cape Sandwich is a bit of nondescript rock at the north end of Hinchinbrook Island. You can imagine the exchange back in the UK

Cook to Lord Sandwich - "Your Lordship, I've called a bit of Oz after you. It's a prominent headland just near the new marina at Cardwell". Lord Sandwich to Cook - "Cheers, Jim!".

Jim was also in such a bad mood that every time things got a bit tense

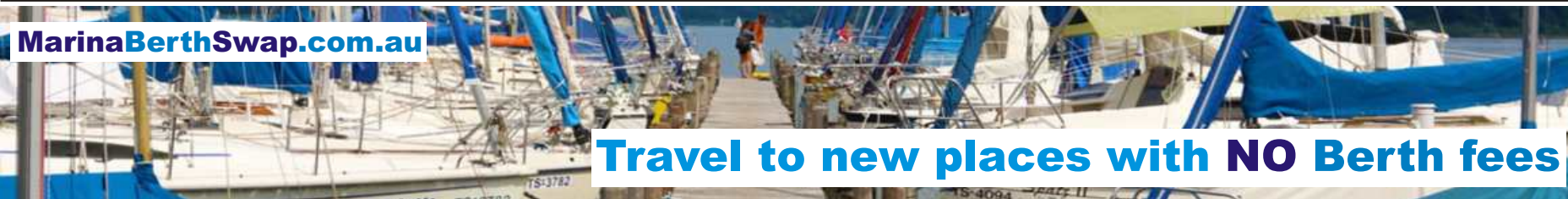
on board *Endeavour* he labelled our coastline with names like Thirsty Sound (no water); Cape Tribulation; Deception Bay; False Cape. Jim must have been the original Whingeing Pom!

Anyway, when he gets up north, Jim finds he's inside the Barrier Reef. (Surprise! All he needed to do was look at his Lucas book to get the true picture.) So he does a no no and sails at night near the Reef and BANG goes up on the bricks. "Oh dear!" says our Jim. He pulls the Endeavour off and goes to Cookie (Cooktown) for repairs.

He finds a big lump of coral stuck in the hull which has acted like a cork in the hole and which kept the whole show afloat. Had the coral piece not been snagged, he would have gone down with the ship and Oz would have been left to the French. Imagine that - we would now be saying "sacre bleu" rather than "bugger"! And Cookie would now be called "Porte Jacques" or similar - not quite the same.

And as everyone knows, Cooktown has been accurately described as "the world's largest unfenced lunatic asylum!". I ask you -does that fit the image of a French Riviera?

continued next page...



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The Cooktown Lighthouse



photo by Marlene Leith

With the *Endeavour* repaired, Jim goes to Lizard Island, walks up the same hill we did on our visit, sees a gap in the Reef and sets off to freedom.

But guess what? He floats out into the open Coral Sea only to get sucked back inside the Reef with the change of the tide neatly positioning him back where he started. Good one Jim! (a second "Oh dear!" from our Jim).

He finally sails to Batavia where a heap of the crew died through mozzie bites (perhaps yet another "Oh dear!" from Jim.) Not quite like in the Indonesian Tourism Corporation "brochure".

So there you have it the real story on Capt James Cook (or as he was at the time Lt James Cook). It was good to follow in his footsteps and we learned a bit of 'istory along the way.

So you see, sailing on *Speranza* is not merely enjoying ourselves with idyllic cruising stuff all the time - it's very educational as well!



Keith & his best mate, Patti

TCP Note: Keith wrote this in 2006 and we are happy to report the Keith, Patti and their cat, Suu Kyi are on their yearly cruise this year.

Cheers Keith and Patti!

Dogs eat anything!



Lea

Photos & story by Susan E. Smith, SY Moonbird

Someone once said "The cat is traditionally ship's companion." Now I know why.....Mackay Marina is rather near two animal shelters. On a whim we decided to visit them both. The first shelter we visited is run by the city, has dogs but doesn't keep them very long. After gazing at the doomed, I left with tears in my eyes, turning away from a small fox terrier cross shaking behind bars. Somehow we ended up at the second shelter that relies on donations where they keep the animals until hopefully homes are found for them. Good grief, what was I thinking? I have been a life long cat person and each puss has trained me well.

They say "Dogs have owners but cats have staff." After the loss of our two cats to old age and electrocution of our five year old grey tabby Violet, I thought; "Maybe a dog would be the perfect pet for us on a boat?" "I love my friend's dogs and you can take them places." Further, I thought, "A dog would deter pirates." In the yard of long rows of cages our eyes simultaneously lit on a little ripper! She was a black and white spotted cattle dog / border collie cross. The next thing I knew we had a dog on board!! Unfortunately she was a sick pup. It was the dreaded parvo virus! We named her "Lea", when in a panic at the symptoms we called the vet and he customarily asked the name of his patient. Anyway, about six hundred bucks later she pulled through and we were off for a three month cruise north.

Right-O. It was clear from the get go that dropping her nose in a litter box was not going to work on the potty training front. This was not like any kitten I ever raised. Clearly I had no chart in my brain for this. We drew straws for who had the privilege of scraping dog doo off the bow deck each morning. In swelly weather we had to lock her in the head with pig ears and toys. Ben thankfully took the lead in training as the experienced "Dog Dad". I poured over "Dog Training for Dummies" and launched into leash training. Suddenly the world revolved around Lea's next walk. I'd never noticed dog people before. Now suddenly I was comparing notes about food, shedding, toileting, toys, etc.

continued next page...

No ONE would recognize me back home with a spotted dog straining down the path and a pocket full of scissored liver snaps in my pocket. Laundering liver snaps is a bad idea... Mr Finnegan, my ever calculating antediluvian domestic short hair would be rolling over in his grave to see this!

Lea settled in, kinda, and spent a lot of time trying to get involved in whatever we were doing. After all as Ben always kept telling me, "We are her pack now and she needs to know who the pack leader is." It wasn't me. I thought I'd won the lottery when I learned to give her a liver snap almost every time she sat. I felt clever and obedient. She likes this game heaps and seemed to be saying, "Oh boy my favourite." In fact anything was; "Oh boy my favourite!"

My life became dog training and keeping everything out of her reach. Weeks were passing and I hadn't painted, spoken to friends without dogs, written anything or done much else. Even taking a shower was a challenge. This was just like human infancy and my children were graduating college and high school. I am in a deep heap of trouble I thought. "It's all about pigs ears and juicy bones?!"

One night it happened. I got tired of being the gear cop. Lea was resting peacefully in her bed on deck, we in our bunk below. Morning arrived and we emerged to find Lea had eaten most of Ben's wallet!! His credit/Atm card had been daintily nibbled beyond use. Bummer, no money. Hmmm %\$@& BAD dog! Ben was not amused. The clerks at the Bank were rolling on the floor laughing. They let him keep the old card as a souvenir and had a replacement to us in five days.

The most depressing thing was when our friend Les said, "It takes about

two years for a dog to grow a brain." Gulp, something had to change here. I think Ben considered a nice spotted rug for the settee area but finally announced "I think we should send her to a farm." Secretly relieved, but also feeling rather sad, off to farm market we went. Ben made a sign and sat there with Lea all day. Finally, a young cattle farmer took her along with two months of crunchies, and anti vermin medicines off in to the sunset, somewhere near Bowen. Lea can now run, play, wrestle and we like to imagine raise lots of spotted pups who wake up each day and say, "Oh boy my favourite!"

As for me, these days I just visit the neighbours cat and water my orchid. Simplicity is a good thing!



Susan

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more on next page...



Vision is for sale



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This vessel has recently undergone an extensive refit and there doesn't appear to be anything left to spend.

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interior. Large furling genoa has been sailmaker inspected and new UV protection strips added. In mast furling main and mizzen sails.

No expense spared refit recently completed with the view of long range world cruising and unfortunately unexpected health problems have put paid to these plans and the boat has reluctantly been placed on the market, at well below cost and well under insured value of \$220,000. Now reduced to \$165,000 this is good value for money. More details on www.nqboats.com.au

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42'6" on deck, 3.6 mtr Beam, 1.8 draft, Samson cutter rigged ketch, centre cockpit. 45hp HRW Lister Diesel, Eutectic fridge, Radar, GPS, Electric anchor winch, 2x225litre fuel, 450litre water, Gas cooker, Hydraulic Steering, TMQ Auto-Pilot, Arco winches, Aquapro dinghy with 4hp Yamaha O/B, 60lb plough anchor on 10mm s/l chain (80m) spare 45lb plough.

New Song is a reliable passage maker and comfortable live aboard yacht. With double bunk, nav station and vanity aft. A "galley alley" leads forward to a spacious saloon forward of which is the head/shower, sail bin and tool area, the forepeak has a vee berth and chain locker. Deck work is easy as most sail control lines lead to the cockpit. I have sailed many miles single handed. We have moved to small acreage, and offer her for sale as a great opportunity for anyone interested in cruising, an extremely satisfying and enjoyable lifestyle. We would also be very negotiable to a buyer who would take her over and use her well.

New Song is currently moored at Iluka, on the NSW north coast. The vessel is probable worth 70k but the asking price is 45k ono, considering the current boat market, and my desire to see her carry someone else on their cruising adventure.

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Build A Catamaran

More pages added November 2015!



This site will feature building projects from as great a variety of materials and build methods as possible. If it's a cat or any project that relates or enlightens we want to feature it here. How about yours??

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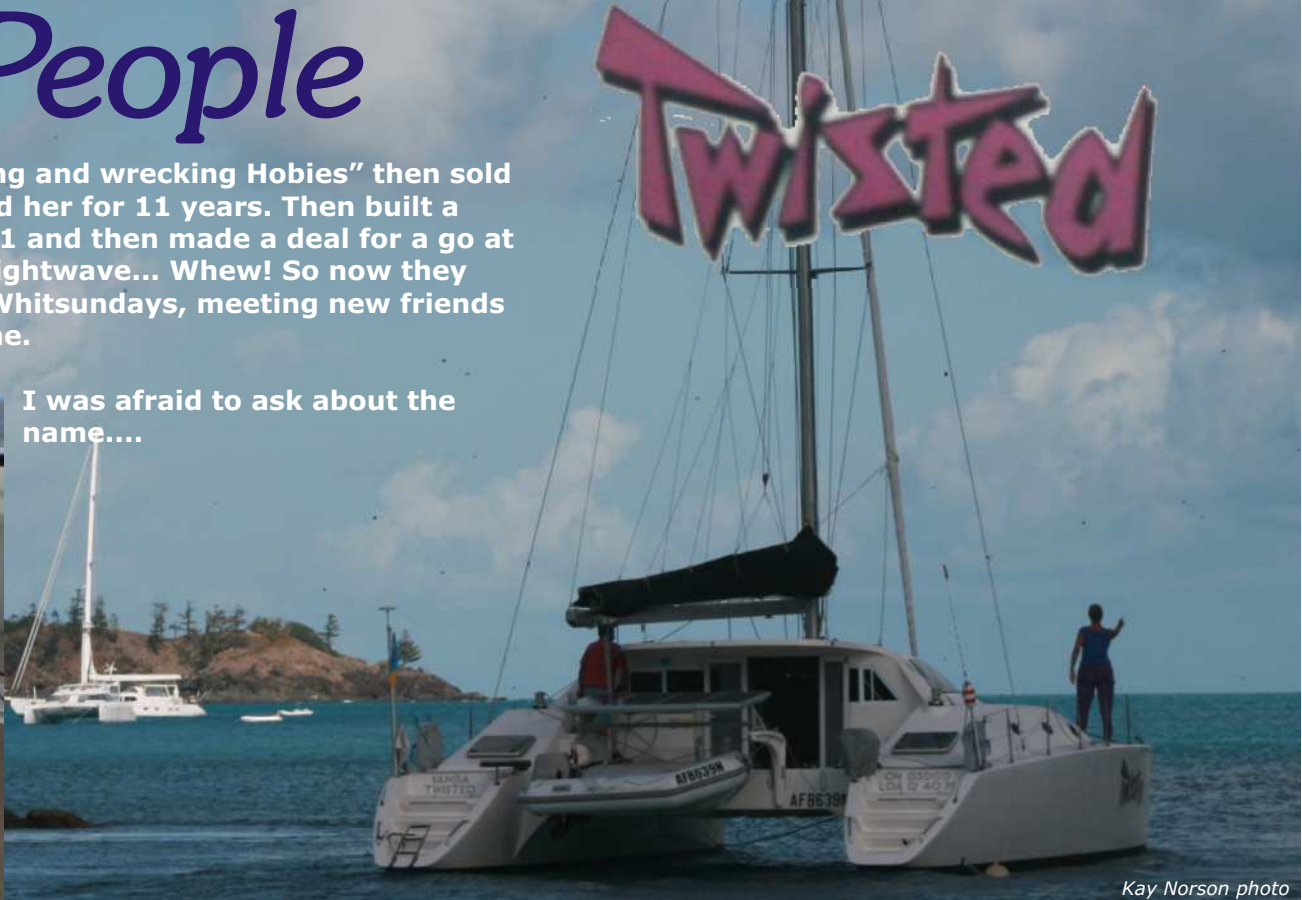
**Deadline for TCP 76:
January 22, 2016**

Passage People

Chris and Margaret started out "racing and wrecking Hobies" then sold the house and bought a Col 35. Sailed her for 11 years. Then built a Grainger Azura 38, then a Farrier F31 and then made a deal for a go at ex Chincogan/Grainger moulds via Lightwave... Whew! So now they are sailing between Yamba and the Whitsundays, meeting new friends and enjoying a very nice 40 foot home.



I was afraid to ask about the name....



Kay Norson photo

Passage People

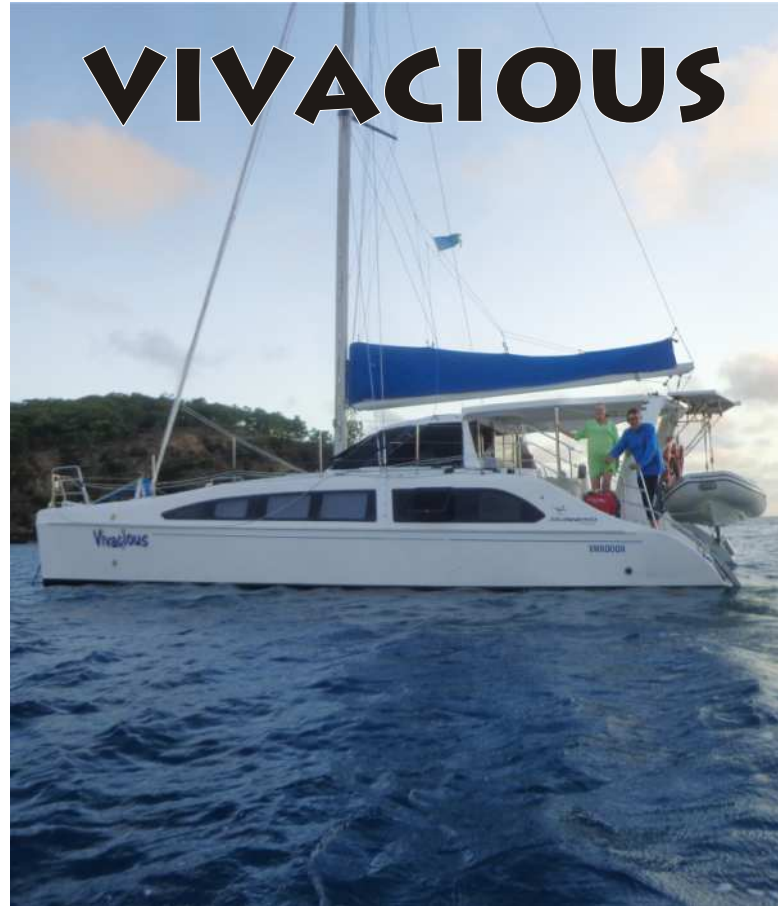


**Ho hum... another beautiful
Seawind 1160...**

Vicki and Rod had either good taste or good advise. They are new to the game, out on their first foray. They come from game stock though, at the Whits they took out Vicki's mum and Rod's auntie, respectively, the ladies into their 80's and 90's! Goodonem!

Rod is a railway man and Vicki is a "home engineer! She is an ex-banker and runs a little real estate business from home as well.

VIVACIOUS



Passage People

Ozzie Mozzie



Peter built **Ozzie Mozzie** around 07-08. The original moulds were made by Bob Burgess and loaned out by Malcome Salisbury, both well known and respected names by those that know multihulls. There were no plans so how to connect the pieces was up to Peter. Looks like he figured it out OK. Just coastal cruising so far but next year...



Pete's secret to success is weight control. He is working on getting a water maker now so he can do home brew. He figures the weight savings from not having to carry enough beer to see him through will make it all pay. How can you argue with that!?

How fast will she go? 20+ is too easy he says.

Maybe ought to do that before beer thirty hey!

Passage People

YANKEE GIRL



Rob was solo sailing for two years and got onto a dating website and found a bride!... in the USA! ... Thus... **Yankee Girl!** But he is still solo sailing, at least for the offshore bits.

Rob is former of *Investigator* and *Trax*, the latter especially, a well known boat around QLD.



Yeppoon is home port and the goal is to set up a little self sufficient place, do a little sailing, do some land travel and take life as it comes... with Debra, the Yankee Girl.

Your boat should always be at least twice the length of your tender... and Rob, you just barely made it buddy!

Passage People



Rachel C



The boat is named in honour of the author of "Silent Spring", the book that first exposed the damage the chemical companies were/are still doing to our planet.

Stu designed and built her from palonia below and ply above the waterline. The 9m cat has twin fully rotating, unstayed masts. Stu has built a couple of boats and dinghies. He likes fixing up old boats and does a delivery or repair when he can. He'll hang about the Whits for a few months and then head back home to Hervey Bay

where he has a bunch of acres up a creek where he can plot his next adventure.. or project.

Larger than life is a term that comes to mind.... live long and prosper mate, And keep on truckin!

Passage People



John (Dad) and Guy (#1 son) and Tim (at left on deck) were piloting a boat load of gear to deliver to the Solomons.

Reverie III



John is a Sydney man and the boys are from Byron Bay. Guy is a sparky with a skippers ticket. They bought the boat in Townsville earlier this year and were on their way north again after sorting out in Ballina.

But I said they were piloting the boat etc... seems in Hervey Bay Guy made a call to his lady back at the Bay. To use the classic term... the rabbit died! Now with a key crew about to make John a grand daddy, what are they gonna do with all those footballs, fishing gear, T shirts, sails.... etc?!

What can we say except ***Congratulations!***

Passage People



Steve

Leanne

Liz Roy

Easy Tiger & Koko

This happy group was easy to get a good people pic but the photographer was having a bad camera day so no usable pics of the Fusion 40 (*Easy Tiger*) or the Elan 37 (*Koko*).

***Koko*:** Roy says the name is from Inuit? Anyway, they were on their way back from Hamilton Island Race Week where they held their own but no big trophies this year. But very interestingly... Roy is used to winning races as we have found another sailor with motor racing background. Back in the day he was the Ducati rider for national motorbike competition. This writer knows how good you have to be to reach that level... having aspired.

Their home port is on the Central Coast, New South Wales.

***Easy Tiger*:** They bought the boat from the original owner/builder who hurt his back and was forced to sell. So the boat started from Bunbury, Western Australia and is now in the process of doing a counterclockwise circumnavigation? Or maybe not. There appears to be a debate in progress. Maybe a step off to PNG or Asia? Leanne says she wants to see a virgin environment. These relatively new sailors bought a outstanding cat and have apparently been managing it well so the world is their oyster. If there is a virgin environment out there they will no doubt find it, and we hope report it for TCP!

Passage People



Dos Gatos!

Dos Gatos is gringo Spanish for *Two Cats*. Being a former gringo, I knew that. Paula and John bought the boat in partnership with other family members three years ago. The South African built Norsman is 15 years old but hardly looks it. Apparently the American former owner kept her in perfect order. *Dos Gatos* is 43 feet long and very beamy as is common on SA built boats. They were taking sailing lessons at the Capricornia Cruising Yacht Club in Yeppoon when they were looking for a boat. When they found *Dos Gatos* she was in Mooloolaba and they sailed her up the coast.

They have a big property on a hill with a view all the way to the marina so they can see if she is still there.



Marodi 2

David began building a Wharram Pahi 41 but realised that with the family about to contract due to kids leaving the nest, he might try a smaller boat. So what to build? A 21? 30? 38?? Finally decided on a modified 30.

David has taken a Wharram and done the unthinkable! He has cleaned up the sheer and has a conventional rig! And didn't stop there, the final blasphemy, a sheltered helm position with wheel steering. Shiver me timbers!



Margarita and David

This is the second trip up the coast from Broken Bay in New South Wales but the first for Margarita. The first trip was so bad David nearly sold the boat. He got caught outside the Gold Coast with an East Coast Low. Very dangerous. He said he was the last boat to get in the Seaway

Passage People



Passage People



The Coastal Passage # 75, Nov-Dec, 2015

Joshua C

This Passage People article should be titled, ***The Fabulous Joshua C*** or some other superlative. TCP doesn't know if there has ever been a great ship like this, built by the owner and sailed the coast. Not that there weren't some big sailers built back in the seventies, ferro and steel mostly, but the economies of ownership and build quality doomed them long ago. But not this one.

She is built of solid GRP. An honest 64 feet on deck, 80 feet LOA and 53 feet of waterline. Sixty tons of real ship. Launched in January 2008.

Her rig is "Bald Headed Schooner" or so Robyn was told but he didn't know what it was called, he just built it to work!

Aside from the anchor windlass, there are no winches on board. Every line is handled on deck. Eight knots is good sailing for her.

So who is this extraordinary team, the man that built her and the very capable first mate that looks so comfortable at the helm??

Turn the page and find out...

Passage People

Robyn is a quiet, proven competent, courageous, very modest gentleman. While in the Percy Lagoon we would see dinghies from the boats anchored in West Bay come in daily and everyone was drawn to *Joshua C* like a magnet. Robyn would usually be asked questions and he would always take a little time. Quiet gentleman.

Annie... she has brass! And a good thing as Robyn would think nothing of putting her on the helm in a tight spot and know she would work it out. Annie is the great socialiser, hostess, navigator and whatever it takes. They make a great team

continues next page...



Passage People

Some background on the builder: Robyn got his first boat at age 23, the 25 foot motorsailer, *Miss Will*. She was powered by a truck motor including the three speed gear box! Nevermind that, he sailed her north from Sydney.

Next was a 38 foot ketch. No name... it was a "cursed boat". Got married, had a kid, lost the boat to the divorce. Where have we heard that before?

Then he built a 41 foot spray in GRP. She was 51 foot LOA, launched in 87 and sold in 93.

Then he got a job building Noosa ferries. Robyn says he was the main builder and finished four of them. That was the finance for *Joshua C*.

There is much more to this story and maybe we can catch up another time. In the meantime, do what everyone else does. When you spot her in the harbour/anchorage, get in your dinghy with your camera and go gawk with your mouth hanging open for a while...



***Joshua C* makes her way out
of the Percy Island Lagoon**

Passage People



James, Sally and Peter were caught out doing *The Joshua C* thing, cameras rolling! So I asked to get a photo of the photographers.

I found out who the clown of the group was. That's Peter on the right below, mugging it up for the camera. This is a group you want to invite to your party!

Nice boat too!



Les Tardis



The Coastal Passage #76 will be out February 2016

Attention TCP readers and advertisers:

Every year we have had to produce a TCP in mid December - the holiday season - for January release. Its always been very difficult to get organized with advertisers and contributors, so we are not going to do that again! February will be the next release date. We also would like to have a chance to enjoy the holiday season not sitting in front of a computer! No edition will be missed or omitted, just reshuffled!

*To all of you boaties everywhere, have a
Happy Holiday Season and New Year!
Cheers, Bob & Kay*

